



TOWN OF BEAUFORT | Entry Master Plan

Prepared by:



STEWART

Prepared for:



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ACKNOWLEDGMENTS

The Residents of the Town of Beaufort

Entry Master Plan Steering Committee

Name	Group
Caryn Stein	Beaufort Housing Authority
Bucky Oliver	Front Street Village
Karl Blackley	Beaufort East Village
Micah Shepard	Beaufort Business Assoc.
Dale Britt	Mainsail Development
Allen Thomas	Marina Development
Reid Smith	NCDOT District Engineer
Lauren Hermley	Citizen
Liz Kopf	Business Owner
Dennis Duffy	Historic Comm. Member
Pat Kenney	National Park Service
Doug Doubleday	Town of Beaufort Planning Board

Town of Beaufort Board of Commissioners

Charles W. Burgess, Jr. - Town Manager

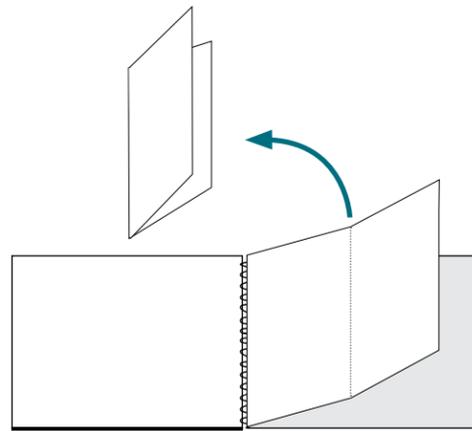
Richard L. Stanley - Town Mayor

Kyle Garner - Town Planner

Executive Summary Booklet

The executive summary can be a stand-alone document. Simply remove this double sided page and fold along the dotted line as shown in the diagram.

To print the executive summary, print pages five and six double sided and fold along the dotted line.



The Entry Master Plan study would not have been possible without the support of the town and the public. Public support will continue to be integral for the successful implementation of the recommended enhancements described within this study. Beaufort's character and authenticity have a history of resiliency through infrastructure changes.

Through strategic proactive planning, public and private participation and a comprehensive framework, Beaufort will remain **"The Coolest Small Town in America"**.



Proposed view looking south on Turner at the intersection of Pine Street

Prepared by:

HadenStanziale

planning
urban design
landscape architecture
civil engineering
environmental graphics

Prepared for:



Acknowledgments:

The Residents of the Town of Beaufort
The Entry Master Plan Steering Committee
Charles W. Burgess, Jr. - Town Manager
Richard L. Stanley - Town Mayor
Kyle Garner - Town Planner

Entry Master Plan | Town of Beaufort

Executive Summary

*full report available on Beaufort Planning website: <http://www.beaufortnc.org/planning-inspections>

Purpose and Process

The purpose of this master plan study is to provide the town with a planning tool for the new Beaufort corridors being caused by the proposed highway 70 realignment. The simple and straightforward recommendations for the new entry corridors are not final plans for development but rather a comprehensive framework for design decisions concerning the future entry experience into Beaufort. It is hoped that the master plan will foster a vision of the developmental, aesthetic, cultural and economic opportunities that the new entries into Beaufort provide and ensure that all future projects will reflect the existing character and charm of Beaufort.

The master plan process focused around public participation. A steering committee was formed representing a cross-section of Beaufort residents and two public meetings were held. The public and the steering committee provided a clear direction for the master plan. A detailed analysis of Beaufort and its "Beaufortness" was presented to the committee and the public. Feedback from these meetings began to shape specific project goals this plan should achieve. A second meeting was held to discuss preliminary design recommendations. Comments from the second meeting were used to finalize the Entry Master Plan. As a comprehensive framework, this report will continue to evolve to meet changing desires and demands and help Beaufort move forward while retaining its scale, character and charm.

Master Plan Goals

The Town of Beaufort established the following focus areas for the master plan: beautification, gateways and wayfinding. These three focus areas represent the major elements that will shape the new entry experience of travelers to and around Beaufort. In order to establish specific goals pertaining to these focus areas, both the steering committee and the public were asked to evaluate what they thought made Beaufort special and what they thought could be improved. Through several steering committee meetings and public workshops the following goals were established:

- 1 Whatever we do should have a sense of "Beaufortness".**
 - Beaufortness is the essence or character of Beaufort.
- 2 Beautification study should focus on the following areas:**
 - Turner / Cedar
 - All 3 town entries
 - 2 district gateways
 - Live Oak / 101 Intersection
- 3 Develop a wayfinding strategy and design character.**



- 4 We want Complete Streets!**
 - bike lanes, sidewalks, trees, vegetation and shade, parking, lights
- 5 Bring Historic Character out to the new entry roads!**
 - Proposed elements should reflect the history of Beaufort
 - Turner Street Bridge lacks Beaufort architectural details but includes desired lighting and sidewalks.
- 6 Cedar Street is a major opportunity!**
 - Decreased traffic on Cedar allows possible reduction of lanes, bike lanes, better sidewalks, parking
 - Improvements should help unify areas to the north and south
- 7 Wayfinding is extremely important!**
 - Turner Street is the entry to Historic District, waterfront and ferry
 - Signs should have a sense of "Beaufortness".

Elements of the Master Plan

The following sections of the Master Plan Report describes the design recommendations for accomplishing the established project goals:

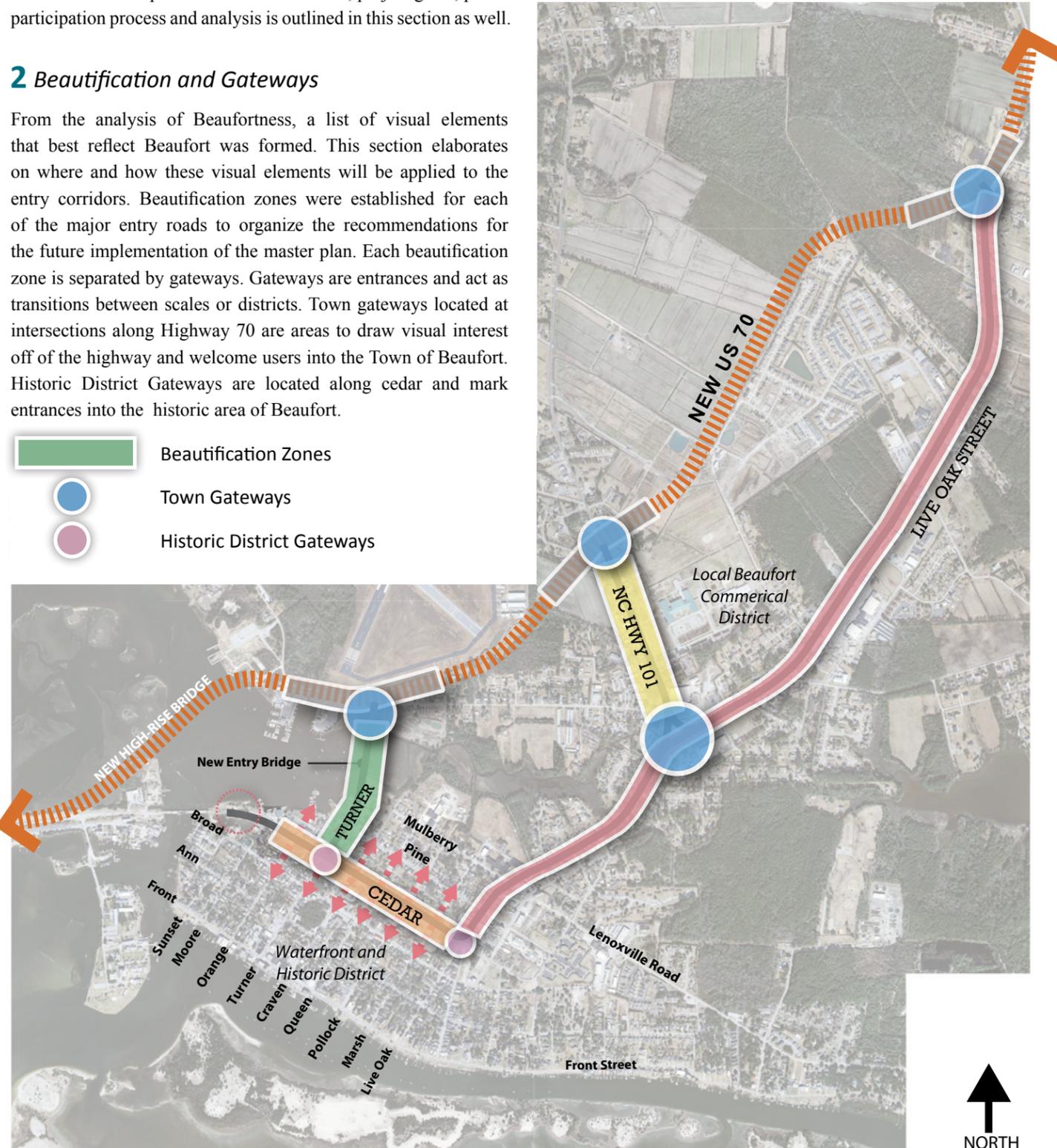
1 Project Overview

This sections analyzes the history of Beaufort, the future infrastructure change, and how to move forward. Infrastructure changes are not new for Beaufort. Bridges have come and gone The realignment of highway 70 is simply the next change bringing new opportunities to the town. An in depth look at the focus areas, project goals, public participation process and analysis is outlined in this section as well.

2 Beautification and Gateways

From the analysis of Beaufortness, a list of visual elements that best reflect Beaufort was formed. This section elaborates on where and how these visual elements will be applied to the entry corridors. Beautification zones were established for each of the major entry roads to organize the recommendations for the future implementation of the master plan. Each beautification zone is separated by gateways. Gateways are entrances and act as transitions between scales or districts. Town gateways located at intersections along Highway 70 are areas to draw visual interest off of the highway and welcome users into the Town of Beaufort. Historic District Gateways are located along cedar and mark entrances into the historic area of Beaufort.

-  Beautification Zones
-  Town Gateways
-  Historic District Gateways



Elements of the Master Plan

3 Wayfinding

The success of the recommendations of the Entry Master Plan are dependent on a clear wayfinding strategy. The steering committee emphasized the importance of wayfinding in the first meeting by stating that,

“Wayfinding signage is the first thing that will set Beaufort apart in scale and character.”

This section focuses on how users will navigate the Town of Beaufort once the new entry corridors are established. The following questions are addressed concerning wayfinding:

- Who are the users?
- Where are they going?
- How do they get there?
- What should the signs look like?

The final portion of this section introduces the potential signage family through a case study of a traveler coming from the west heading to the historic district. As travelers enter from either the east or the west along the new US 70, appropriate signage will direct users to their specific destinations. Once users exit off the new US 70 through a town gateway further signage will inform and direct users where they are, where they should go and how to get there. After users have located parking, pedestrian scaled signage will guide users to their destination. Because Beaufort is such a walkable town, pedestrian scaled signage is important to facilitate walking from one destination to the next.



4 Implementation and Action

The final section discusses how to move elements of the master plan from a vision to reality. The following five focus areas for implementation are elaborated on:

- Public Awareness/ Education
- Design
- Fund-Raising
- Maintenance
- Regulatory Controls/Enforcement

Implementation of this Master Plan will require a significant commitment on the part of many public entities, private corporations, civic groups, and individuals.

This section concludes with immediate and long term action items based on recommendations of the master plan. It is critical that pilot project be undertaken by the town to encourage participation from all people. Some of the action items suggested are not extensive and can easily be accomplished in the immediate future.

1 Section 1 PROJECT OVERVIEW

History Shaping the Future

Infrastructure changes are not new to the Town of Beaufort. Since the town was established in 1709 numerous infrastructure changes have occurred, but the character and essence of the third oldest town in North Carolina has stayed the same. Many elements of Beaufort that have been established since the road grid was laid out in 1713 have not changed. The historic authenticity of Beaufort is what continues to draw visitors today. Until 1907 Beaufort was a secluded fishing town.

“While isolation gave Beaufort a genuine sense of place, connection with the rest of the continent gave it a way to grow.”

– “Beaufort-by-The-Sea” - Rick and Marcie Carroll

The train or “The Mullet Line” connected Beaufort to the rest of North Carolina by the first bridge over Gallants Channel to Broad Street. “The Big Ditch”, otherwise known as the

Atlantic Intracoastal Waterway, brought and continues to bring seafaring visitors from all around the region. Travelers on land entered Beaufort from the north on NC-10 (current NC-101). Beaufort’s courthouse was the terminus of the Great Central Highway, NC-10. NC-101 originally followed the current US-70 alignment to Morehead City. In 1927 with the coming of US-70 to Beaufort, the first road bridge crossed Gallants Channel to Ann St. As technology and industries evolved rail services were abandoned and the road entry into Beaufort shifted from Ann to the current entrance at Cedar Street built in 1957. Beaufort’s history, charm and realness has continued to draw visitors and residents. The proposed new alignment of Highway 70 is yet another infrastructure change bringing new challenges and opportunities to the town.



Post Card of Ann Street - early 1900's



Train Comes to Beaufort - 1906



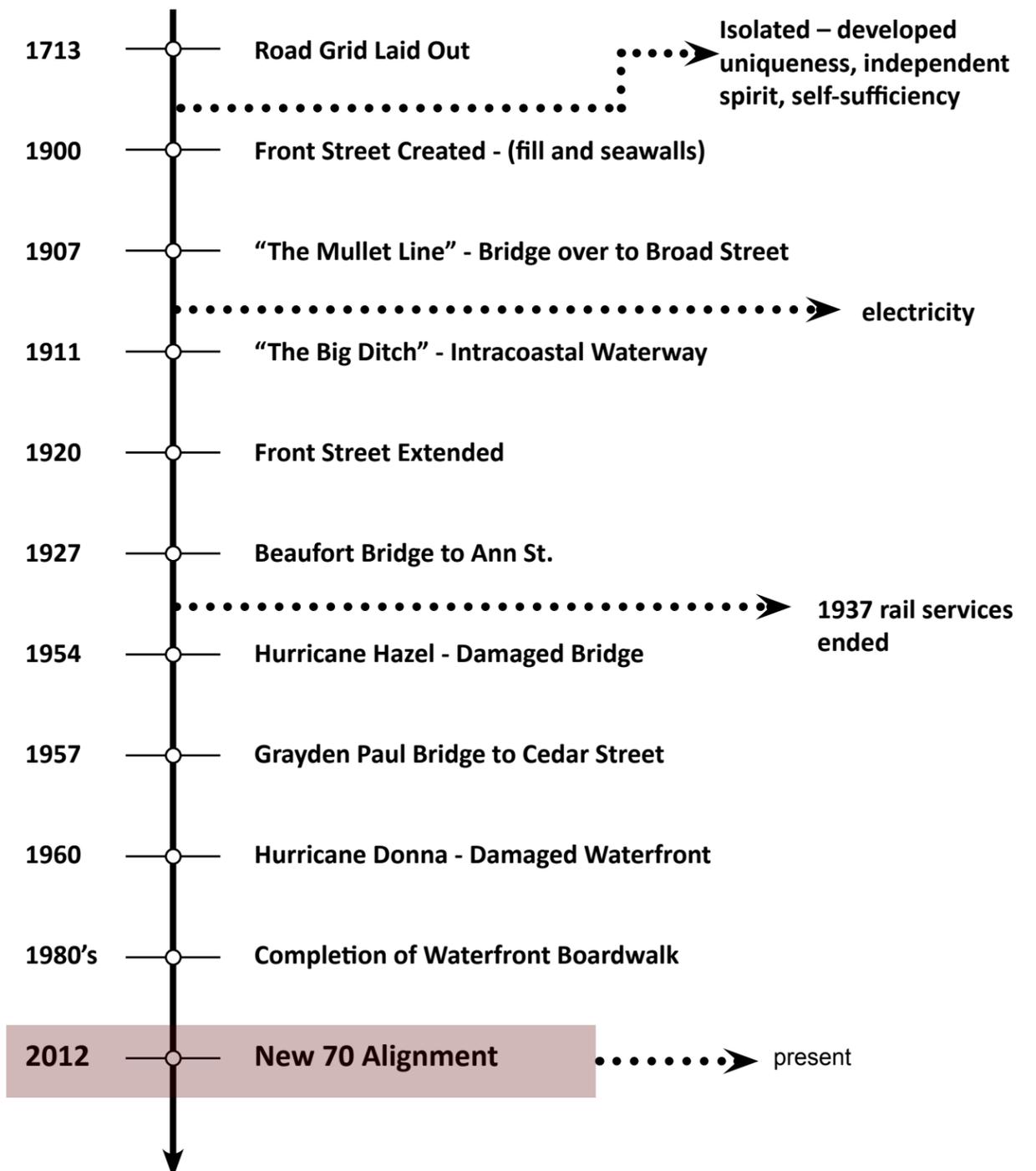
Front Street - 1911

image source: “Beaufort: An Album of Memories” by Jack Dudley



Beaufort Bridges - late 1950's

Past: Timeline of Historic Infrastructure Changes



History Shaping the Future

Present: Beaufort Entries

The existing entry into Beaufort across the Grayden Paul Bridge was established in 1957 following Hurricane Hazel in 1954 which washed out the Ann Street bridge foundations. The relocation of the town entry to Cedar helped to preserve many homes and trees along Ann Street that would have been affected with the needed road modifications due to traffic. Commercial development along Cedar Street and Live Oak Street has increased as a result of the shifted entrance and increased traffic. Although development has occurred along Cedar Street, Front Street continues to draw businesses and visitors. Since 1957, the sounds of the current two-lane draw bridge signal the arrival to Beaufort.

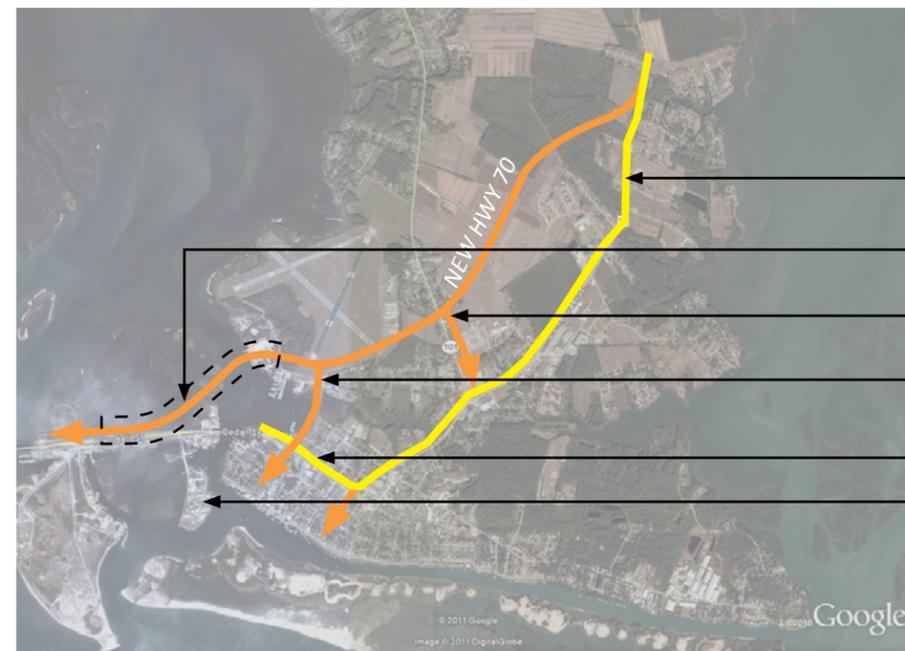


Current 70, Cedar Street entrance into Beaufort

GRAYDEN PAUL BRIDGE

Future: Beaufort Entries

Looking towards the future of Eastern North Carolina, the North Carolina Department of Transportation saw it necessary for a new high-rise bridge and re-alignment of the existing Highway 70 corridor. Increased tourism and residents of areas east of Beaufort required assessment of traffic volumes, evacuation routes and general transportation in the eastern part of the state. The new highway 70 will split from the current route just before Pivers Island. A new fixed span bridge will extend over Gallants Channel to a location just south of the airport. The new entrances into Beaufort will change dramatically turning south on Turner Street to the Waterfront. NC-101 and Live Oak Street will also experience increased traffic from the bypass to the waterfront.



Proposed New Highway 70 and Bridge

LIVE OAK STREET

PROPOSED FIXED SPAN BRIDGE

NC - 101

TURNER STREET

CURRENT HWY 70 (CEDAR STREET)

PIVERS ISLAND

Moving Forward

Focus Areas

The Town of Beaufort saw that with change comes the chance to make positive enhancements to the entry experience of Beaufort. Many major changes have come to Beaufort but the essence of the town has remained the same. To ensure the new Beaufort entries would be appropriate in both scale and character with the existing town, and that Beaufort would be prepared for the future changes that the new alignment of Highway 70 will bring, the town pro-actively sought to create a master plan to guide the creation of the new Beaufort gateways and corridors. The purpose of the Beaufort Entry Master Plan is to provide recommendations for the following components:

Beautification - what will be the visual character of the entry corridors and gateways?

Gateways - where are appropriate gateways into Beaufort and how should they be treated?

Wayfinding - how will people navigate to and around Beaufort and what will the wayfinding elements look like?

The focus areas of the master plan include:

Turner Street

Turner Street will act as the new entry corridor into the Historic District of Beaufort. This corridor includes the new proposed Turner Street bridge.

NC - 101

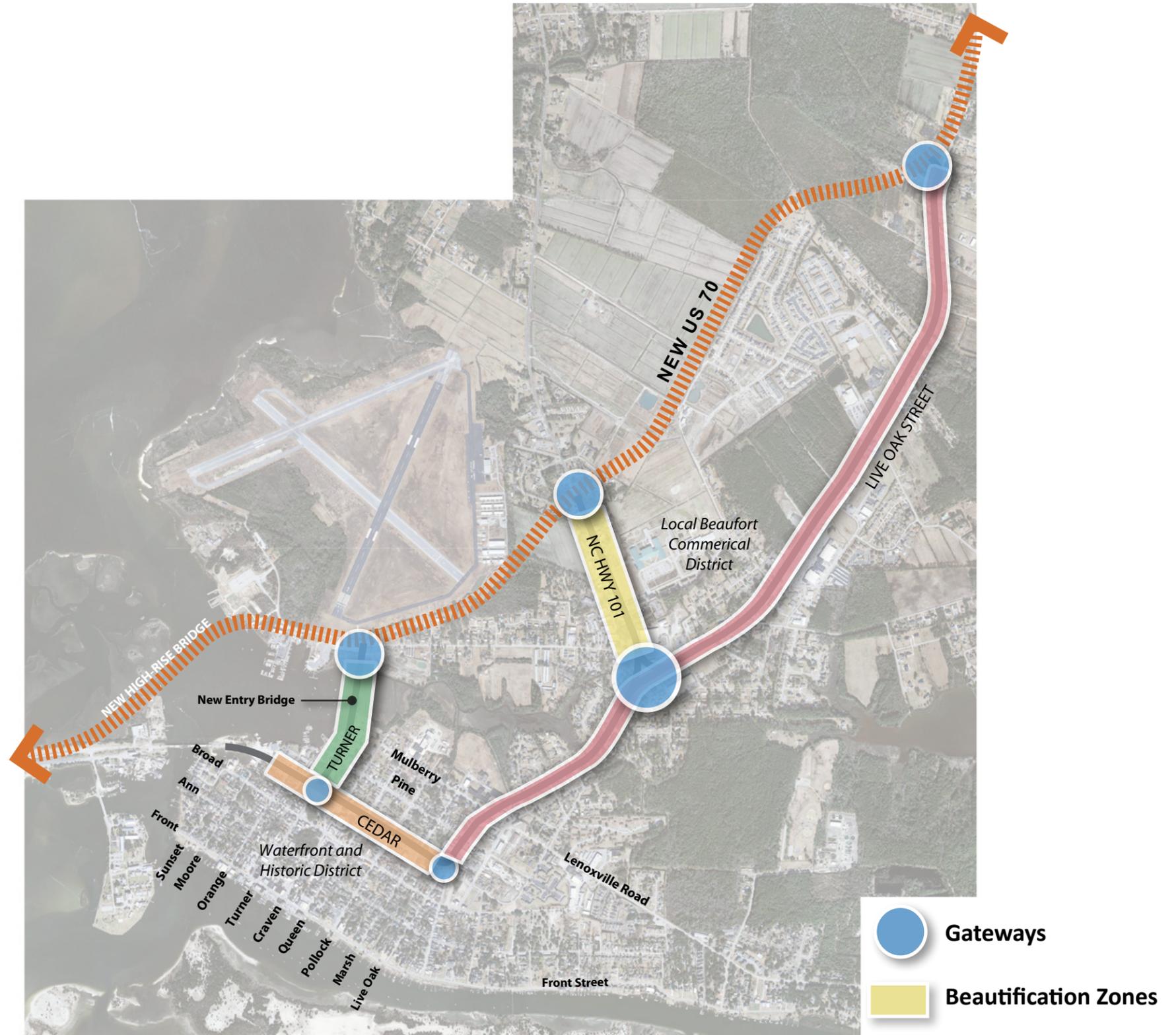
NC-101 will be a major entrance into the Town of Beaufort for locals and commercial traffic. The entry corridor will be anchored with two gateways at the New 70 and Live Oak intersections.

Live Oak Street

Live Oak Street will remain an important street due to the necessary services and local business it serves. It also will be a connector street to the Historic District.

Cedar Street

Cedar Street will no longer function as highway 70 and therefore will take on a new look and feel. Although traffic on Cedar will be reduced, it will still remain an important street in Beaufort.



Location map of gateways and corridors study area

Steering Committee

In order to better understand the concerns and desires of the citizens of the town, a steering committee was formed. The steering committee represented a cross section of Beaufort citizens. Members of the committee ranged from concerned citizens to State and Federal representatives to business owners. The steering committee advocated for what they saw would be the best for Beaufort and its citizens. Regular steering committee meetings were held to discuss recommendations based on the focus areas provided by the Town of Beaufort.

Steering Committee	
Name	Group
Karl Blackley	Beaufort East Village
Dale Britt	Mainsail Development
Doug Doubleday	Town of Beaufort Planning Board
Dennis Duffy	Historic Comm. Member
Lauren Hermley	Citizen
Dan Kelly	Citizen
Pat Kenney	National Park Service
Liz Kopf	Business Owner
Bucky Oliver	Front Street Village
Micah Shepard	Beaufort Business Assoc.
Joe Siegler	Citizen
Reid Smith	NCDOT District Engineer
Caryn Stein	Beaufort Housing Authority
Allen Thomas	Gallants Channel Marina and Land Owner

Committee Meetings and Process

The following is an overview of the process and meetings as well as general results of the steering committee (see Appendix: Meeting Minutes for details of each meeting):

Kick-off Meeting

- This was the first meeting with the committee. An overview of the project and initial thoughts were discussed. Overall project goals were established.

Analysis and Synthesis Meeting

- Findings of an in depth analysis of the history, character and wayfinding of Beaufort was presented. From this meeting visual elements that best reflect Beaufort were identified.

Public Meeting Recap

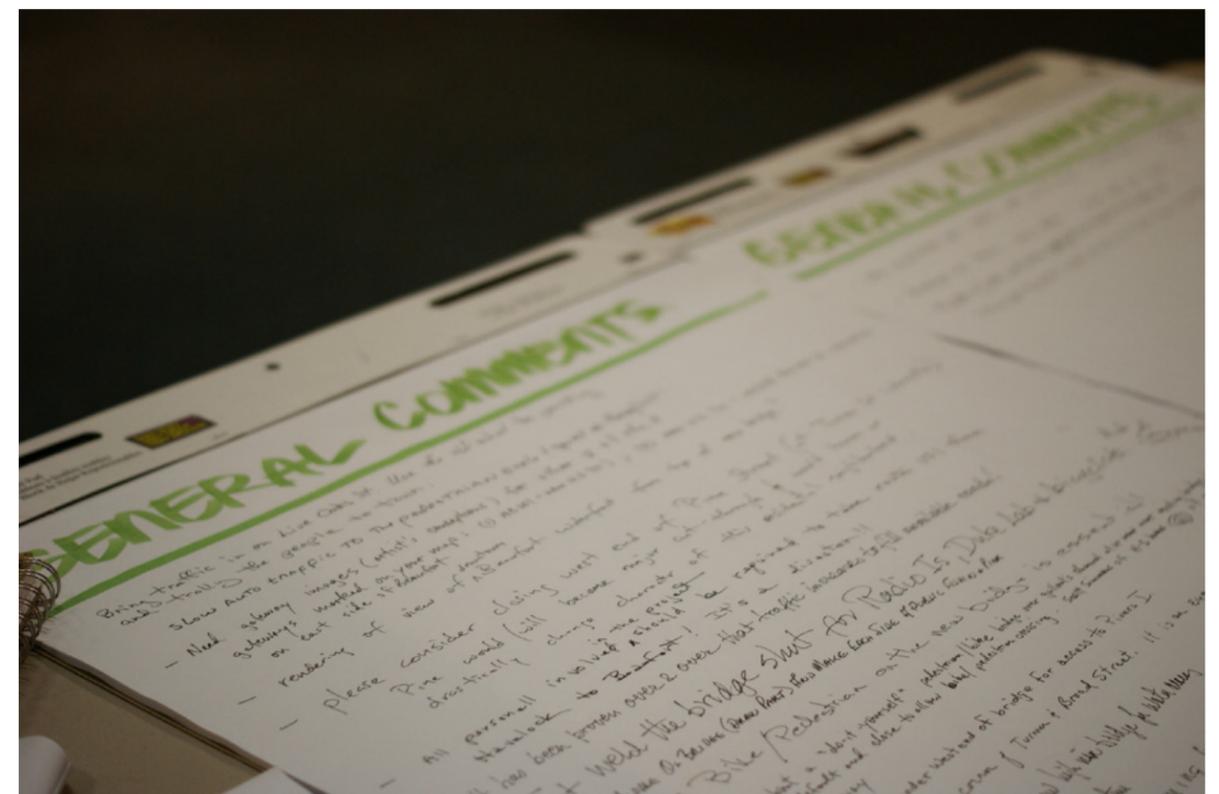
- Following the first public meeting, the committee reviewed the thoughts and concerns of the public. Next steps were discussed and a second public meeting was scheduled.

Public Meeting Recap

- The second public meeting focused on preliminary recommended enhancements for the entries and gateways into Beaufort. A recap meeting with the committee was held to reflect on the public's feedback and to discuss implementation strategies and an action plan for the master plan recommendations.

Final Comments

- The committee reviewed a draft copy of the master plan and final revisions were made based on comments received from the steering committee.

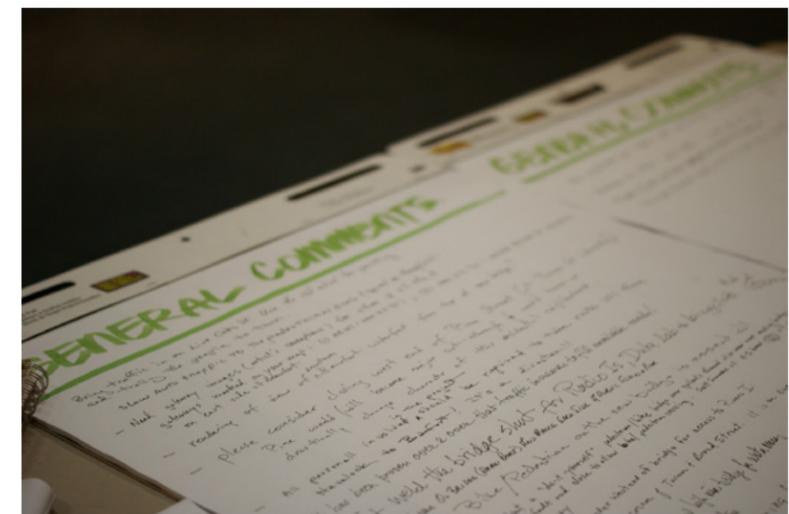


Public Participation Workshops

Public workshops were an integral part of the master plan process. Two public participation workshops were conducted. The purpose of the meetings was to receive public input that would inform design decisions.

The first meeting centered around the three main focus areas outlined by the town: beautification, gateways and wayfinding. Participants were asked to list words that best described Beaufort and then locate where in Beaufort those items or descriptions were found. From this exercise it was found that citizens consider Beaufort friendly, unique, simple and natural. These characteristics were seen primarily within the historic district. For the second exercise, pictures of existing and NCDOT proposed streets and intersections were shown to the public. Citizens were asked to list what they liked and disliked about each image. This exercise provided a better understanding of citizen's opinions concerning what they thought best visually exemplified Beaufort. The third series of exercises focused on wayfinding. From these exercises participants provided locations of important destinations within Beaufort, preferred routes to the listed destinations and preferred visual characteristics of Beaufort signage.

Comments were synthesized from the steering committee and the public into recommendations for beautification, gateways and wayfinding. A second public meeting was held to present preliminary designs to the public. Final input of the master plan recommendations was gathered and used to refine any outstanding issues within each focus area. Participation from the public informed each process of the master plan. It also allowed the public to become educated along the way about what changes are coming and how those changes could affect them. The meeting successfully spread the word to the community about the potential future changes while also allowing the public to shape the look and feel of the Town of Beaufort.



Beaufortness Analysis

The common goals between the steering committee and the public focused on preserving the scale and character of Beaufort. The steering committee created a word to express this idea - “**Beaufortness**”. To assure the new Beaufort entries reflected the scale, character and charm of existing Beaufort, it was necessary to first learn from existing Beaufort. Therefore; a “Beaufortness” analysis was conducted to help determine what characteristics people truly loved about Beaufort.

“It does not appear that Beaufort... wants or needs to be anything other than what it is – a three-century-old waterfront town.”

- “North Carolina Beaches” - by Glenn Morris

“Beaufortness” Characteristics

From initial research, including conversations with residents, site visits, a visual inventory and a historical investigation the following characteristics were found to best described Beaufortness.

- 1 Human Scaled**
 - proportions of buildings, streets and the landscape
- 2 Evolved Variety**
 - Changes due to infrastructure modifications and natural forces
- 3 Common Materials and Furnishings**
 - Unifying unique physical elements of Beaufort.

“Beaufortness” Precedent Study

In order to inform what “Beaufortness” means for the new entry corridors, a precedent analysis was performed for key streetscapes in the town. The steering committee agreed that characteristics of Beaufortness were best represented on Turner, Ann and Front Street.

- *Turner Street* - Due to the variety of uses, best represents a mixed-use street.
- *Front street* - is the precedent for a commercial street based on the concentration of businesses located along the waterfront.

- *Ann Street* - represents an example of a residential street that reflects the characteristics of Beaufortness. Elements from these streets will help direct the development of the entry corridors.

Turner Street will be a new entrance into Beaufort and will continue to develop as a mixed-use street. Cedar Street’s traffic volume will be reduced due to the new 70 alignment but will continue to serve as a commercial street in the future. Live Oak north of Cedar will connect Historic Beaufort to the commercial district and remain a mixed-use street. South of Cedar street on Live Oak will follow the residential precedent for development.



Front Street
commercial

Turner Street
mixed-use

Ann Street
residential

Dashed lines represent entry corridors studied within the master plan. The color corresponds to the precedent street.

Front Street - Commercial Precedent Street



An analysis of Front, Turner and Ann Street were conducted. Front Street's analysis is shown here as an example of what elements were discovered through the study. Reference the beaufort analysis presentation in the appendix for the complete report.

From an analysis of Front Street, numerous elements that make Beaufort unique and authentic can be seen. Elements fall within the three categories of "characteristics of Beaufortness". The architecture is one aspect that reflects Beaufortness. Buildings are one to two story with varying architectural styles. Wide sidewalks provide a safe area for pedestrians. Wood is used extensively for signs, posts and benches. Overall Front Street provides plenty of handcrafted visual interest on a pedestrian scale.

Human Scaled
 1-2 Story Buildings
 2 Lane Road
 On-Street Parking
 Wide Sidewalks
 Pedestrian-oriented Signs

Evolved Variety
 Building Architecture
 Varied Building Height
 Varied Streetscape
 Varied Parking

Materials + Furnishings
 Unique Entries
 Wood - Signs, Posts, Benches
 On-street Parking
 Wide Sidewalks



Turner Street - Mixed-use Precedent Street



Ann Street - Residential Precedent Street

"Beaufortness" Elements

To implement Beaufortness along the corridors it is necessary to first identify what are the essential elements that best reflect "Beaufortness" and second to delineate appropriate locations for each element. Through meetings with the steering committee it was clear that

"Anything we do should have a sense of Beaufortness"

From further meetings with the public and the steering committee the following chart was developed. It outlines the preferred visual elements and where those elements should be located on the new entries. The overlapping preferences fall under the shared category. Expressed preferences exclusive to the committee or the public are shown under their respective column. The public's preferred visual elements for each entry corridor provided the foundation to create entry designs that would meet the needs of the town. As you will see in the following section, each entry corridor includes the visual elements preferred by the public.

Preferred Visual Elements	Cedar (Mixed-Use)	Turner (Mixed-Use)	NC - 101	LIVE OAK	SHARED	COMMITTEE ONLY	PUBLIC ONLY
street trees	•	•	•	•	•		
historic/night-sky lights	•	•			•		
tasteful signage	•	•	•	•	•		
green space	•	•			•		
strategic parking	•	•			•		
underground powerlines	•	•			•		
uncluttered, safe sidewalks	•	•	•	•	•		
upgraded site furnishings (ie, trash receptacles)	•	•				•	
planting strips for street trees	•	•			•		
roundabouts	•	•	•	•	•		•
beaufort fences	•	•			•		
places to view water and natural environment	•	•			•		
access to water	•	•			•		
portals to water	•	•			•		
adapted use of homes	•	•			•		•
bicycle lanes	•	•	•				•
handcrafted signs	•	•	•	•			•

2 Section 2 BEAUTIFICATION AND GATEWAYS

The Entry Master Plan

Utilizing the goals set forth by the committee and the public and the findings of the Beaufortness analysis, the master plan will serve as a guide for the establishment of beautification and gateways. Beautification is organized through the establishment of different zones. Each zone contains preferred visual elements as agreed upon by the steering committee and the public. The zones help define districts.

Gateways are located at transition points between zones and at key entries into Beaufort. There are two types of gateways; historic district gateways and town gateways. Town gateways are located at all entrances off of the New 70 and at the intersection of Live Oak Street and NC-101. Each town gateway will signal the arrival into the Town of Beaufort. The historic gateways located at the intersection of Turner and Cedar Street and the intersection of Live Oak and Cedar Street will signal the entrance into the Historic District within the Town of Beaufort.

The following section provides “Before and After” illustrations of each beautification zone and gateway. The proposed designs and recommendations represent the desires and preferences of the citizens of Beaufort.

[click to jump to page](#)

New 70 Beautification will accent the major gateways into Beaufort with wildflower plantings, NCDOT signage on “Beaufort Posts” and Beaufort columns.

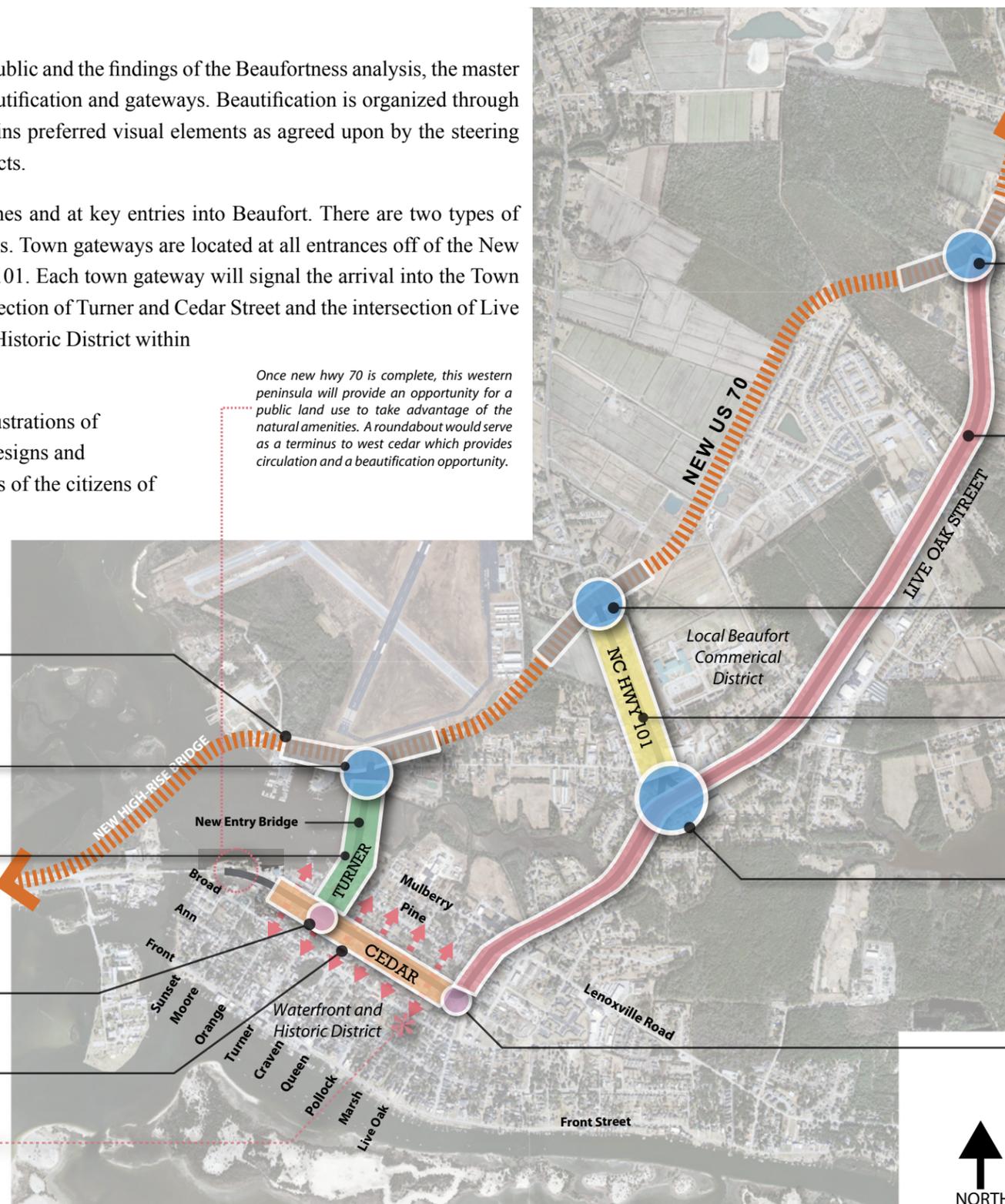
This gateway will welcome visitors to Beaufort once they have crossed the new Hwy 70 bridge and will direct visitors to Turner Street for access to the Historic District.

Turner Street Beautification Zone includes the new Turner Street Entry Bridge which will serve as the Historic District Gateway. The “beaufortness” character of Historic Beaufort should be applied to this zone as it will serve as the main entry to the Historic District.

This gateway will reflect the “beaufortness” of historic Beaufort and welcome visitors to the Historic District zone.

Cedar offers a major opportunity to “take back” the street and utilize it as a **viable link** between the historic district and the areas north of cedar. Cedar Street will no longer serve as the major vehicular route through Town. Cedar should now serve as a major local street reflecting the character of Beaufort while accommodating pedestrians bicyclists, parking and automobiles.

Once new hwy 70 is complete, this western peninsula will provide an opportunity for a public land use to take advantage of the natural amenities. A roundabout would serve as a terminus to west cedar which provides circulation and a beautification opportunity.



NCDOT’s re-alignment project will change the vehicular routes into Beaufort and the scale of the new entry roadways. The goal of this Master Plan is to transition from the scale and “look and feel” of the new Hwy 70 to the scale and “look and feel” of the Town of Beaufort.

The town gateways will serve as welcoming points of entries to Beaufort for travelers on new highway 70. The Live Oak gateway will serve as a direct access to the local Beaufort commercial district.

Live Oak serves as one of two access corridors to the local commercial district. The beautification of this corridor will require public/private partnerships to be formed to beautify areas both inside and outside the right of way, such as street tree programs and building facade upgrades.

The town gateways will serve as welcoming points of entries to Beaufort for travelers on new highway 70. The 101 gateway will serve as a direct access to the local Beaufort commercial district.

101 serves as one of two access corridors to the local commercial district. The beautification of this corridor will require public/private partnerships to be formed to beautify areas both inside and outside the right of way, such as street tree programs and building facade upgrades and public sidewalk initiatives.

This critical intersection of 2 major vehicular corridors provides a major opportunity to utilize this circulation node to provide beautification in the local commercial district and a gateway to the historic district. Current traffic issues can be remedied through the use of a roundabout which also provides a focal point for beautification.

This intersection serves as one of the major entry points into the waterfront/historic district and requires significant infrastructure upgrades to bring the character of historic Beaufort to this gateway.

[click to go back to Master Plan](#)

| NCDOT plan

New 70 / Turner Intersection **NEW 70 BEAUTIFICATION ZONE**



NCDOT view looking east from the new bridge to the Turner Street intersection

Public Meeting Feedback Summary

Things the Public Liked

- Will be able to see Historic Beaufort from new bridge
- Street lights on bridge are nice
- Visible Drainage

Things the Public Didn't Like

- No sign for "Ferry Service"
- No sign for "Cape Lookout National Seashore Gateway"
- Sign doesn't specify Beaufort as Historic
- No bike lanes or pedestrian paths!!!!!!
- No Trees or landscape!!
- No Museum sign
- Does not look Historic!!
- Too wide and too modern!
- Lacks transition to slow traffic

The proposed profile of the New Highway 70 from the NCDOT seems sterile and over scaled. Landscape enhancements along the highway are minimal consisting of seeded grass. The intersection of Turner and the New 70 will be signalized. Proposed signage will direct users to "Beaufort" and "Tourist Info".

- 1** Turner Street Bridge - standard DOT bridge with metal railings
- 2** no proposed vegetation
- 3** proposed DOT signage
- 4** metal pole mast arm signalized intersection
- 5** grass median



Key Map

| recommended enhancements

New 70 / Turner Intersection **NEW 70 BEAUTIFICATION ZONE**



Proposed view looking east from the new bridge to the Turner Street intersection

Approaching the Turner Street and New Highway 70 intersection, travelers will know that they are at a Beaufort town entrance based on the landscape and signage. Wildflowers are recommended to replace the NCDOT’s specification of turf as a beautification element along 70 at all Town of Beaufort entrances. Reestablished native plantings are recommended beyond the shoulder where further land disturbance may disturb the existing vegetation. Simple painted white wooden posts are recommended to replace the existing NCDOT wood sign posts. The signal metal pole mast arms could be painted to dissipate the stark metal feel of the galvanized posts.

- 1 Modified Turner Street Bridge
- 2 Proposed Wildflower shoulder
- 3 Re-established native plantings
- 4 Wildflower median
- 5 NCDOT Sign on “Beaufort Post”
- 6 Painted metal pole mast arms



Key Map



NCDOT view of proposed Turner Street Bridge looking south towards Cedar Street

Public Meeting Feedback Summary

Things People Liked

- Boardwalk is gone

Things People Didn't Like

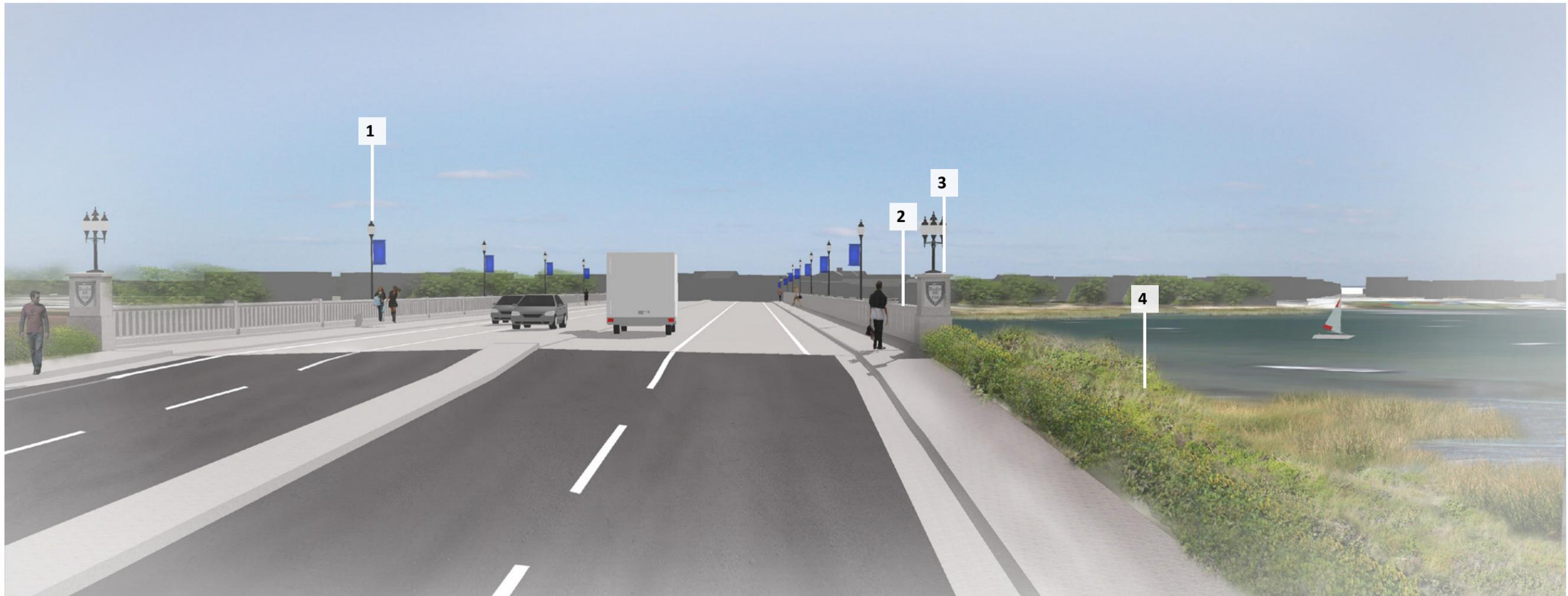
- Bridge does not look like Beaufort!!!!
- No street trees or landscape
- Boardwalk is not preserved
- No bike lanes!!

The proposed profile of the Turner Street Bridge from the NCDOT contains typical elements of most bridges across North Carolina like metal railings, concrete medians and tall vehicular-scaled lights. Sidewalks are proposed on both sides of the bridge but no dedicated bike lanes are shown. The bridge does not reflect the charm and character of Beaufort. Landscape enhancements along Turner Street are minimal consisting of seeded grass.

- 1 Turner Street Bridge - standard DOT bridge with metal railings
- 2 standard DOT street light
- 3 no proposed vegetation



Key Map



Proposed view of Turner Street Bridge looking south towards Cedar Street

The Turner Street Bridge and surrounding area will function as the Town Gateway. It is recommended that the bridge leading into Beaufort from Turner be modified from the proposed NCDOT Bridge to better reflect the character and history of Beaufort. Columns with the Beaufort crest have been proposed at the ends of the bridge to usher travelers into Beaufort. Decorative lighting with banner graphics will further reinforce the arrival into the town. Re-established native vegetation along the sides of the bridge will help integrate the road with the scenic views of Town’s Creek.

- 1 historic pedestrian-scaled street light with banner
- 2 concrete railings to match old entry bridge
- 3 accent column with beaufort crest at ends of bridge
- 4 re-established native vegetation



Key Map

|existing

Turner Street

TURNER BEAUTIFICATION ZONE



Existing view of Turner Street looking south towards Cedar Street at the intersection of Pine Street.

Turner Street north of Cedar Street currently is a two lane road with mostly residential development. Street trees are sporadic. There is a wide planting area between the five foot sidewalk and the road on both sides of the street. The total right-of-way from the edge of the sidewalks is 60'. The road seems exposed and unkept.

- 1 existing 2-lane street
- 2 planting area with no trees
- 3 overhead power lines
- 4 existing five foot sidewalks on both sides



Key Map



Proposed view of Turner Street looking south towards Cedar Street at the intersection of Pine Street.

Turner Street will function as the primary entrance into Historic Beaufort and therefore must reflect the pedestrian scale and character of Beaufort. The two-lane street profile with a turning lane will remain but lane widths of 11' as opposed to 12' are recommended. Bike lanes are not proposed on the road. A dedicated 10' wide multi-use path is recommended to run from the Turner Bridge to the waterfront. This direct link to the waterfront will provide a safe connection for all non-motorized users. Five foot planter strips with native canopy trees are proposed along Turner to provide needed shade and reduce the scale of the road corridor. A six foot sidewalk is proposed on the east side of Turner. All of these modifications will remain within the existing 60' right-of-way. Buried or consolidated power lines are recommended to reduce visual clutter and allow adequate room for trees to grow.

- 1** 2-lane street with center turn lane
- 2** five foot planter strip with street trees
- 3** buried overhead power lines
- 4** proposed 10' bicycle and pedestrian path
- 5** beaufort fence to define properties.
- 6** pedestrian navigational sign



Key Map

|existing

Turner / Ann Intersection **TURNER BEAUTIFICATION ZONE**



Existing view of Turner Street looking south towards Front Street at the intersection of Ann Street.

The existing view of Turner Street looking south towards Front Street is void of street trees. The road profile is wide with parking on both sides giving an exposed feeling. Sidewalks exist on both sides with a small grass strip separating the sidewalk from the street. Parking is located on the corner and is un-screened and un-shaded.

- 1 existing five foot wide sidewalk
- 2 existing on-street parking on both sides
- 3 planting strip with grass
- 4 wide street profile
- 5 existing large surface un-shaded parking lot



Key Map

| recommended enhancements

Turner / Ann Intersection

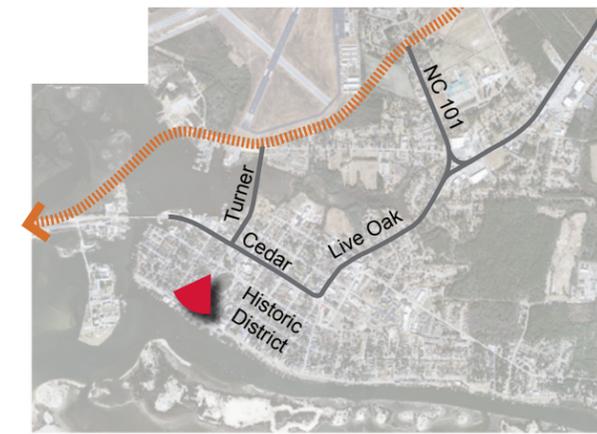
TURNER BEAUTIFICATION ZONE



Proposed view of Turner Street looking south towards Front Street at the intersection of Ann Street.

Simple modifications along Turner Street south of Cedar will help beautify and enhance the entry experience down to the waterfront. The 10' wide multi-use path will continue town towards the waterfront. On-street parking along Turner will remain. At all intersections bulb outs have been recommended. Bulb-outs are extended planting areas that extend to the edge of the parallel parking. They will provide additional areas for landscape opportunities and reduce the crosswalk distance, making for a safer and easier pedestrian experience. All sign posts are recommended to be painted white to keep with the character of Beaufort. Beaufort fences are recommended to screen any surface parking lots adjacent to the street. Crape Myrtles or similar low growing trees are recommended at intersections and along Turner.

- 1 enlarged 10' wide bicycle and pedestrian use path
- 2 on-street parking to remain
- 3 planting strip to remain, bulb outs added at intersections
- 4 street trees added at bulb outs
- 5 parking to be screened using beaufort fence
- 6 white painted sign posts



Key Map



Existing view of NC-101 looking south towards Live Oak Street.

The existing NC-101 road is busy with local and commercial traffic coming to and from local and regional cities to the north. Residential and agricultural uses are prevalent along this corridor. Bike lanes or enlarged shoulders are nonexistent.

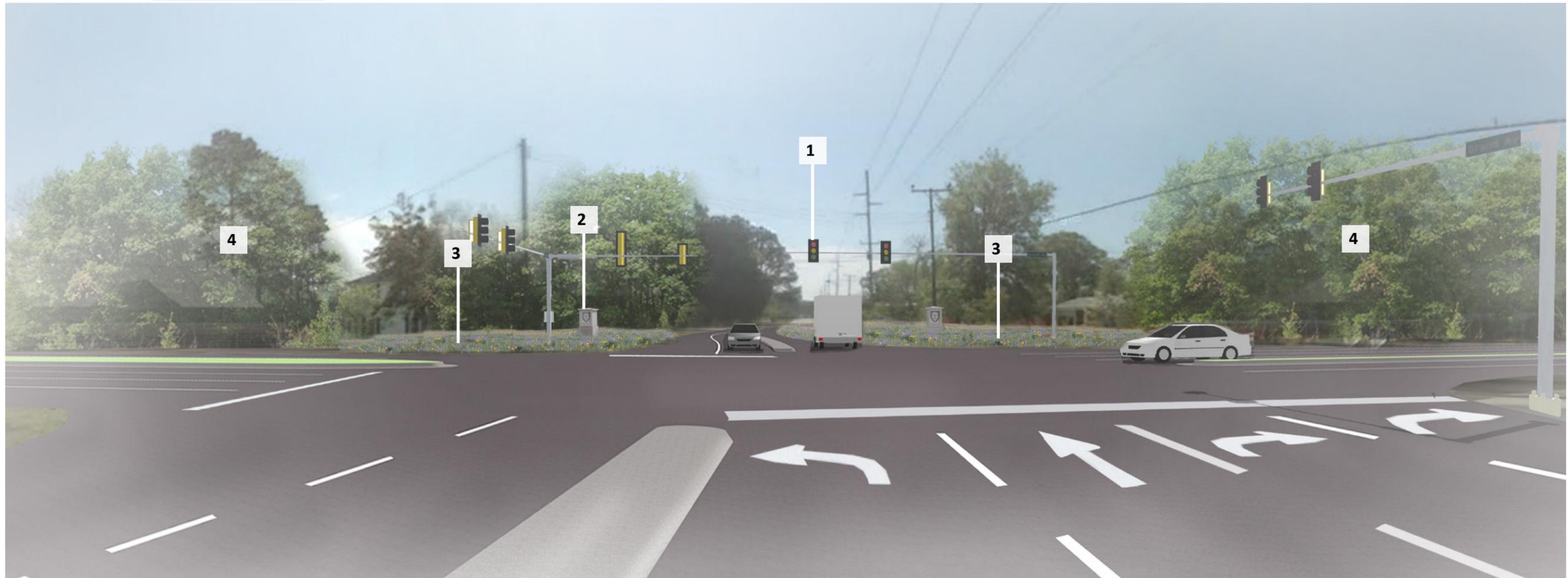
- 1 existing NC 101
- 2 existing homes
- 3 existing vegetation



Key Map

| recommended enhancements

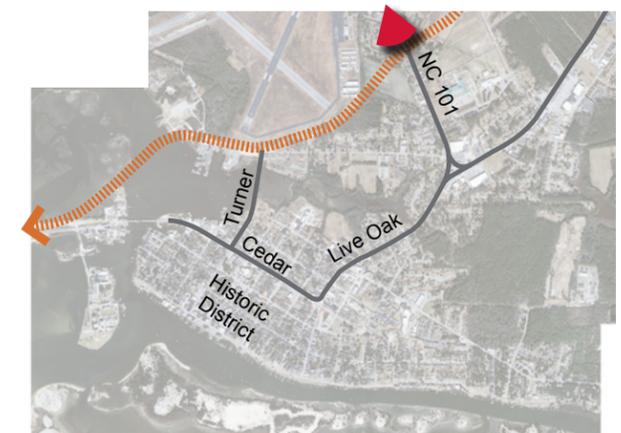
NC - 101 / New 70 **TOWN GATEWAY**



Proposed view of NC-101 looking south towards Live Oak Street at the intersection of New Highway 70

The intersection of NC-101 and New 70 will function as a major entrance for local and commercial traffic. It is recommended that this large signalized intersection have similar elements as the entrance at Turner Street. Wildflowers have been recommended along the edges of NC-101 and New 70 near the intersection to add visual interest. Beaufort columns with the Beaufort Crest will help reinforce the entrance into Beaufort. Preservation or re-establishment of existing vegetation is recommended around this town gateway.

- 1 new signalized intersection
- 2 Beaufort entry columns used at all entry roads off of the new 70
- 3 wildflower beds
- 4 protected existing vegetation



Key Map

|existing

101 / Live Oak Intersection **TOWN GATEWAY**



Existing view of NC-101 looking south towards Live Oak Street intersection.

Public Meeting Feedback Summary

Things People Liked

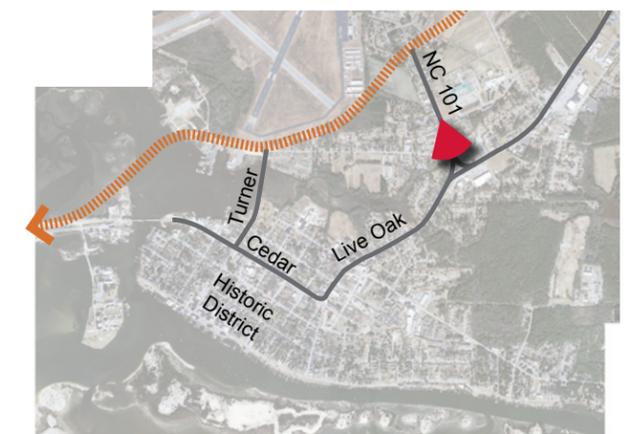
- Existing Traffic flow of traffic entering Beaufort from 101 to Live Oak to Cedar
- Nothing!!

Things People Didn't Like

- Traffic Problems!!!!
- Driveway entrances and exits are too close to intersection!
- No bike lanes or pedestrian paths!!!
- Visual clutter!!
- No landscaping
- Does not look Historic!!
- Too wide and too modern!
- Lacks transition to slow traffic

The intersection of NC-101 and Live Oak is dominated by asphalt and concrete. Entrances and exits of businesses and residences are large and not clearly defined. Due to the over abundance of paving there are few areas for landscaping. The intersection is large and disorganized.

- 1** undefined entries
- 2** unnecessary paved medians
- 3** confusing intersection.
- 4** multiple driveway entries near intersection



Key Map

| recommended enhancements

101 / Live Oak Intersection

TOWN GATEWAY



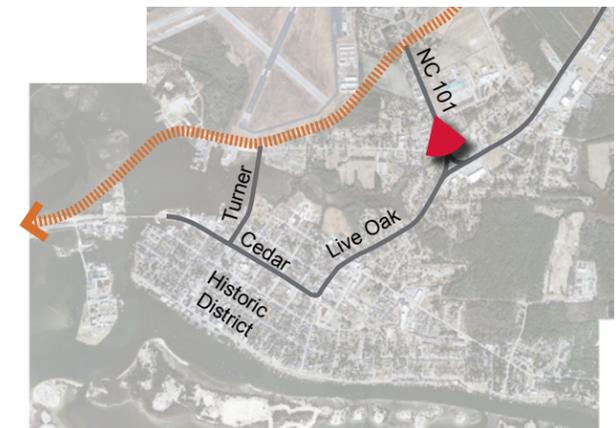
Proposed view of NC-101 looking south towards Live Oak Street intersection.

The existing intersection at NC - 101 and Live Oak is notorious for being confusing and dangerous. The recommendations of this master plan simplify the traffic pattern with the implementation of a single lane roundabout and provide more opportunities for landscape beautification. It is recommended that business and residential entrances be simplified and defined. By defining and reducing the entrance areas to surrounding business, existing concrete and asphalt could be converted to gateway beautification. Lower commercial signs will help reduce the amount of visual clutter. The center of the roundabout provides a location for an iconic landmark.

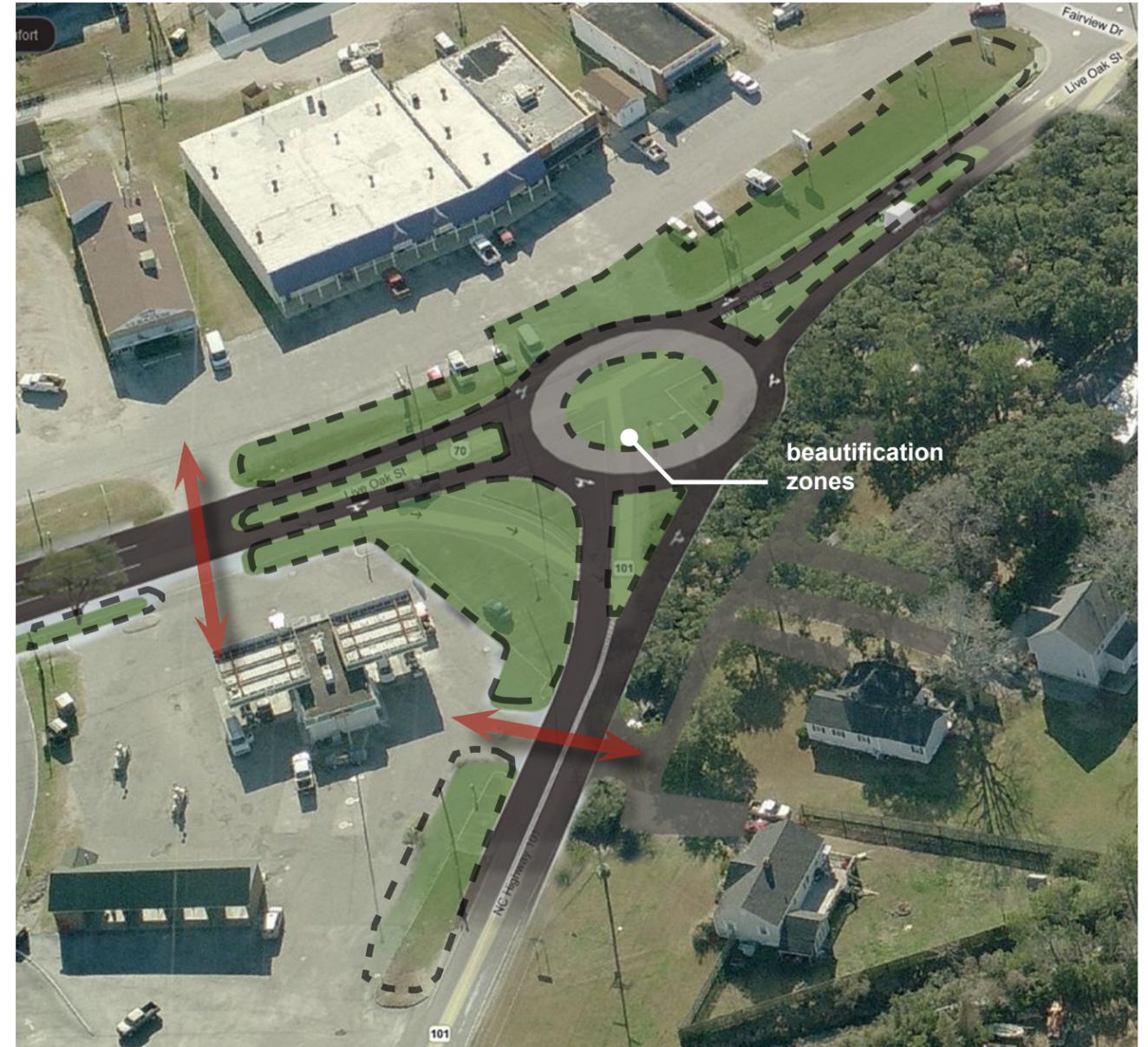
NC-101 BEAUTIFICATION ZONE

Small changes to NC-101 will enhance the entry experience along this highly used corridor. Native street trees are recommended along NC-101 to the Town Gateway at Live Oak. Sign posts along this corridor are recommended to be painted white. To define property boundaries, white Beaufort fences are recommended.

- 1 simplified intersection to a single lane roundabout
- 2 art installation space in center of roundabout
- 3 defined entry to gas station
- 4 lower commercial sign
- 5 simplified driveway entries
- 6 "Beaufort Fence" added to define property boundaries.
- 7 modified white sign posts
- 8 street trees



Key Map



Current Issues:

- Undefined business entries
- Multiple driveway entries near intersection
- Confusing traffic pattern
- High number of conflict points
- Lack of Beautification Elements

Recommendations:

- Defined business entries
- Centralized driveway entries away from intersection
- Simplified traffic pattern
- Low number of conflict points
- Multiple Beautification Zones

| recommended enhancements

New 70 / Live Oak Intersection

TOWN GATEWAY



- 1 new non-signalized intersection
- 2 Beaufort entry columns
- 3 wildflower beds
- 4 protected existing vegetation
- 5 bike lane and connecting path to neighborhoods



Key Map

Proposed view of Live Oak and New Highway 70 looking southwest down highway 70.

The Live Oak entrance at New 70 will be the first entrance to the Town of Beaufort for travelers from the east. It is recommended that this entrance be treated with the same elements as NC-101 and Turner entrances. Beaufort columns, wildflowers and protected/re-established vegetation will help signal the entrance into the town. As proposed by NCDOT, the entrance at Live Oak Street will be non-signalized. Many citizens have raised concerns about the safety of this intersection. It is recommended that alternative traffic configurations be considered. If no alternate traffic pattern can be agreed upon, then a bike and pedestrian link from Shell Landing and surrounding neighborhoods to Live Oak is recommended.

LIVE OAK BEAUTIFICATION ZONE

Along the Live Oak Street entry corridor there are several beautification enhancements recommended. Visual clutter due to numerous street signs is uninviting. It is recommended that commercial signs be reduced and lowered in order to enhance the entry experiences. Street trees are recommended where possible. Bike lanes along Live Oak are recommended to provide a safe means of travel from many of the surrounding neighborhoods to the daily services located along this corridor.

|existing

Cedar / Turner Intersection

HISTORIC DISTRICT GATEWAY



- 1 layers of overhead power lines
- 2 existing traffic signal
- 3 existing 4 lanes of traffic
- 4 ornamental grass planting strip



Key Map

Visual clutter dominates the landscape along Cedar Street. A web of power lines crisscross the street. Unofficial signage at intersections is prolific. The four lane street is buffered on either side by ornamental grass planting strips. Numerous curb cuts exist along Cedar for businesses and residences.

Existing view of Cedar Street looking west towards Turner Street intersection

| recommended enhancements

Cedar / Turner Intersection

HISTORIC DISTRICT GATEWAY



Proposed view of Cedar Street looking west towards Turner Street intersection

- 1 simplified power lines to one side, locate remaining power lines underground
- 2 street trees
- 3 enlarged planting area
- 4 bike lanes
- 5 street narrowed to two lanes
- 6 New 70 Bridge
- 7 traffic circle to replace signalized intersection
- 8 pedestrian navigational sign



Key Map

The intersection at Cedar and Turner will serve as the gateway into the historic district for most visitors to Beaufort. It is recommended that Cedar Street be reduced to two lanes due to the reduction in traffic. Replacing the signalized intersection with a simple one-lane roundabout could provide a visual gateway into the town. With the reduced road profile there would be additional room for bike lanes, street trees and buried power lines - all of which will enhance the entry experience.

|recommended enhancements

Cedar / Turner Intersection

HISTORIC DISTRICT GATEWAY



Proposed view of Cedar Street looking west towards Turner Street intersection

- 1** simplified power lines to one side, locate remaining power lines underground
- 2** street trees
- 3** enlarged planting area
- 4** bike lanes
- 5** street narrowed to two lanes
- 6** New 70 Bridge
- 7** new signalized intersection
- 8** bulb outs at intersection
- 9** pedestrian navigation sign



Key Map

As an alternative to a roundabout, a signalized intersection with bulb-outs is recommended. The bulb-outs will reduce the cross-walk distance making for an easier and safer pedestrian experience as well as adding areas for beautification.

Cedar / Turner Intersection Roadway Intersection Options



Bulb Outs:

- More Parking Opportunities
- No additional right of way needed
- Bulb out areas provide additional planting zones
- Signalized light
- 3 lanes on Turner approaching Cedar.



Roundabout:

- Entry marker for Historic District
- Minimal right of way acquisition needed
- 2 lanes on Turner approaching Cedar
- Planting zones in center and along Turner median
- Current entry drives must be modified

|existing

Cedar Street

CEDAR BEAUTIFICATION ZONE



- 1 layers of overhead power lines
- 2 existing intersection
- 3 existing 4 lanes of traffic
- 4 ornamental grass planting strip
- 5 parking lot adjacent to street
- 6 existing street light



Key Map

Existing view of Cedar Street looking east.

Public Meeting Feedback Summary

Things People Liked

- Decorative lights on left
- Vegetation

Things People Didn't Like

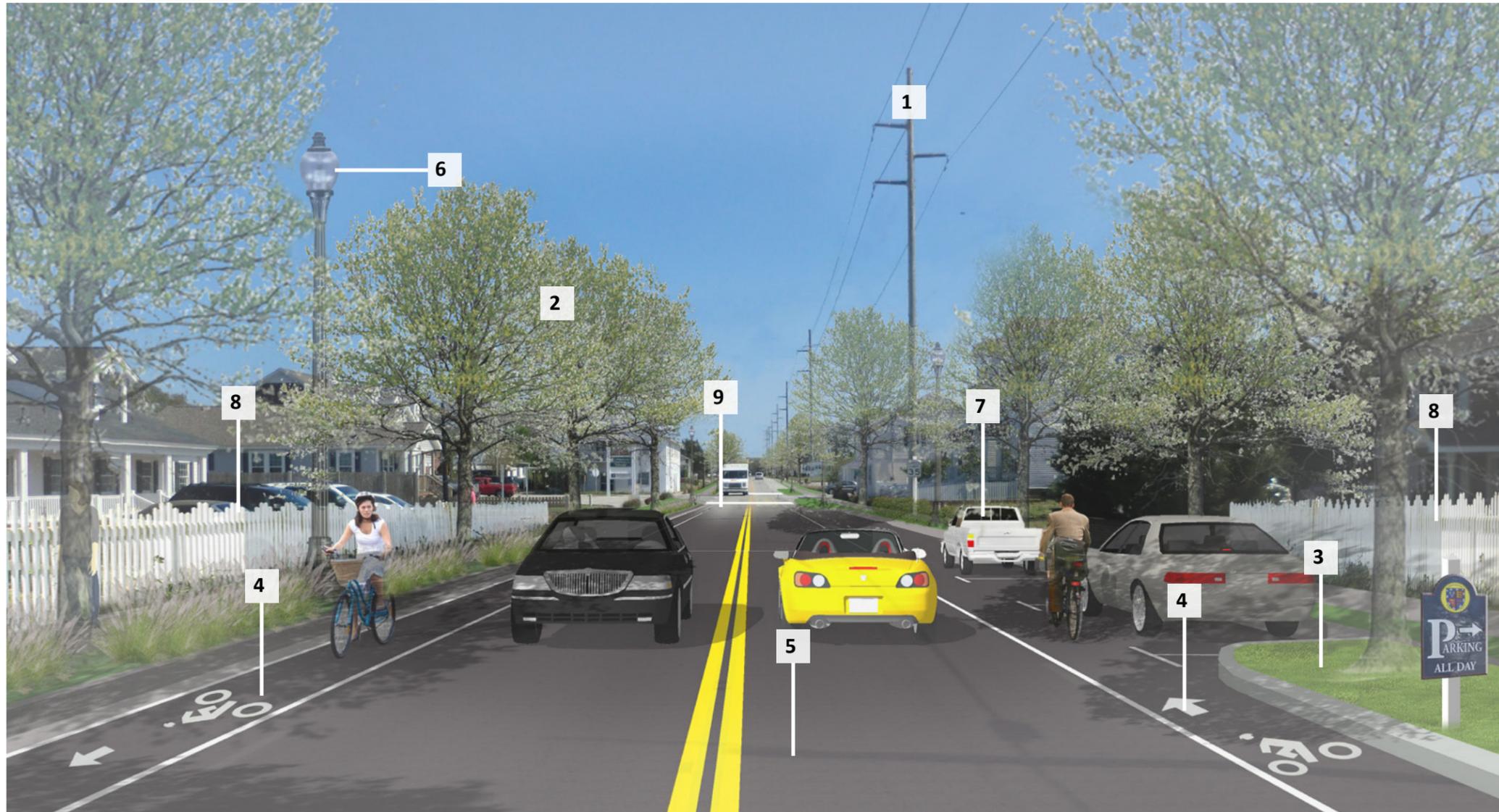
- Too wide for proposed traffic flow- convert to two lanes!!!
- No bike lanes or pedestrian paths!!!!
- Not enough landscape – 2 lanes with more planting areas!!!
- Light pollution
- Power lines!!!!
- No street trees!!
- Road condition
- Cedar Street as “Bus. 70”
- No style or charm

The existing four lane Cedar Street is busy with a high volume of traffic. As seen along the entirety of Cedar Street, the view is dominated by layers of overhead power lines. Because of the abundance of power lines, Cedar Street has no street trees and therefore lacks shade.

| recommended enhancements

Cedar Street

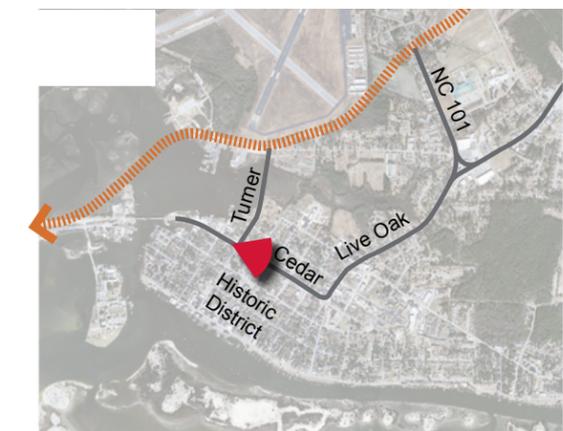
CEDAR BEAUTIFICATION



Proposed view of Cedar Street looking east.

Cedar Street provides Beaufort the opportunity to “Take Back the Street”. Due to the reduction in traffic it is recommended to reduce the existing four lane street down to two lanes. It is recommended that Cedar Street become a complete street, accommodating all forms of transportation. Needed parking spaces will be added along Cedar. Burying or consolidating the power lines is recommended in order to provide the opportunity for installing larger canopy trees and decorative lights. Existing parking lots are recommended to be screened with Beaufort fences.

- 1 simplified power lines to one side, locate remaining power lines underground
- 2 street trees
- 3 enlarged planting area
- 4 bike lanes
- 5 street narrowed to two lanes
- 6 replace lights to match historic street lights
- 7 proposed on-street parking
- 8 “Beaufort Fence” added to screen parking and define edge of properties.
- 9 enhanced crosswalks to strengthen pedestrian connection between areas north and south of Cedar



Key Map

|existing

Cedar / Live Oak Intersection

HISTORIC DISTRICT GATEWAY



Existing view of Cedar Street and Live Oak intersection looking southeast.

The intersection of Live Oak and Cedar Streets is a busy one. Crossing the streets can be difficult. There are no handicap ramps at the crosswalks which limit the accessibility of Cedar and Live Oak. Buildings are set back from the street with parking areas lining the edges of the intersection. The intersection feels large and uninviting for pedestrians.

- 1 layers of overhead power lines
- 2 existing traffic signal
- 3 existing 4 lanes of traffic
- 4 no planting area adjacent to road
- 5 parking lot adjacent to street



Key Map

| recommended enhancements

Cedar / Live Oak Intersection

HISTORIC DISTRICT GATEWAY



Proposed view of Cedar Street and Live Oak intersection looking southeast.

The intersection of Cedar and Live Oak Street will serve as the historic gateway on the east side of Cedar. A roundabout is proposed to anchor this gateway and provide a sense of entry into the historic district. It is recommended that signage be added at this intersection welcoming users to the Historic District of the Beaufort. Enhancements recommended along Cedar Street will continue to the intersection of Live Oak. Enhanced plantings and street trees are recommended.

- 1 simplified power lines to one side, locate remaining power lines underground
- 2 street trees
- 3 enlarged planting area
- 4 street narrowed to two lanes
- 5 replace signalized intersection with traffic circle
- 6 "Beaufort Fence" added to screen parking
- 7 Historic Beaufort Entry Sign



Key Map

3 Section 3 WAYFINDING

Introduction

Wayfinding is the process of helping people navigate to a desired destination through the use of various strategies including signage, the most common strategy.

Wayfinding is a critical component of the Beaufort Entries Master Plan because the way people navigate to Beaufort and its various destinations will completely change with the Highway 70 re-alignment. Visitors and residents will no longer cross the Grayden Paul Bridge, or enter Beaufort on Cedar Street. Cedar Street will become a local street to access the historic district and areas to the east. Unification and organization of message is critical to ensure clarity when navigating Beaufort.

In order to create the Wayfinding Master Plan, input was garnered from both the Steering Committee and the Public to help answer four (4) fundamental wayfinding questions:

- Who needs directions?
- Where are they going?
- How should they get there?
- What should wayfinding elements look like?

In regards to signage, the Steering Committee realized the importance of wayfinding signage as reflected in their following two (2) statements:

“Wayfinding signage is the first thing that will set Beaufort apart in scale and character.”

“Whatever we do should have a sense of Beaufortness”

The following Wayfinding Master Plan Summary includes the thoughts and input of the Steering Committee and the Public and answers the above four (4) fundamental questions.



Users

The Steering Committee and Public agreed that there are a myriad of people who need directions to and around Beaufort, however, most users fall into 3 categories: tourists/visitors, ferry-goers and local traffic.

WHO NEEDS DIRECTIONS?

Tourists

Ferry go-ers

Local Traffic

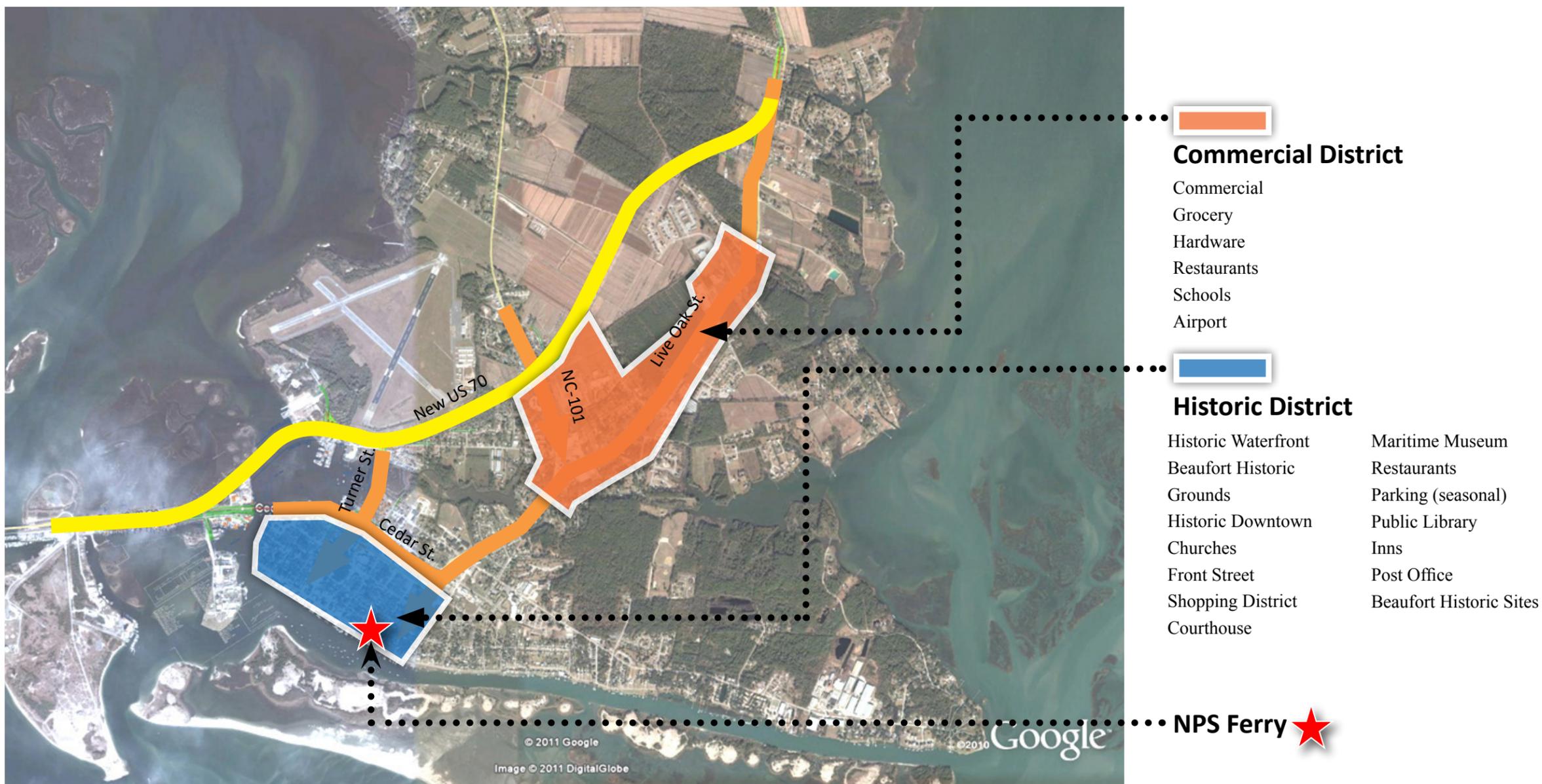


Destinations

The Steering Committee and Public agreed that the 3 major user groups are ultimately traveling to 3 major destinations.

These destinations can be further organized into major districts within Beaufort allowing for the simplification of information that will be placed on signage.

WHO NEEDS DIRECTIONS?	WHERE ARE THEY GOING?
Tourists	Historic District
Ferry go-ers	Ferry
Local Traffic	Commercial District



Routes

The Steering Committee and Public agreed that the 3 major user groups should be directed to distinct entrances depending on the ultimate destination.

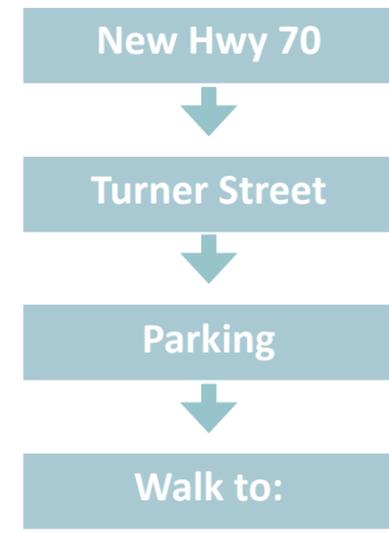
Deliberately directing users to certain destinations will allow for more direct access to the places people wish to go and predictable traffic patterns.

WHO NEEDS DIRECTIONS?	WHERE ARE THEY GOING?	HOW WILL THEY GET THERE?
Tourists	Historic District	Turner Street
Ferry go-ers	Ferry	Turner to Front Street
Local Traffic	Commercial District	Live Oak and NC-101



 **Historic District**

Wayfinding Strategy



- Historic Waterfront
- Beaufort Historic Grounds
- Historic Downtown
- Front Street
- Shopping District
- Courthouse
- Maritime Museum
- Restaurants

Routes

The Steering Committee and Public agreed that ferry go-ers should be directed to Turner Street from Hwy 70.

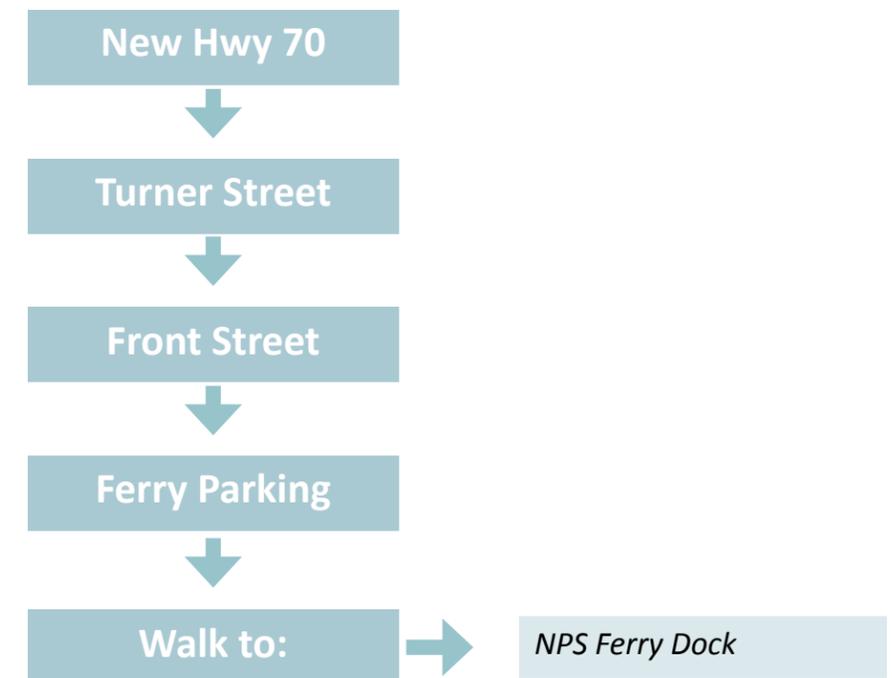
This will allow direct access to Front Street and ultimately the ferry parking area.

WHO NEEDS DIRECTIONS?	WHERE ARE THEY GOING?	HOW WILL THEY GET THERE?
Tourists	Historic District	Turner Street
Ferry go-ers	Ferry	Turner to Front Street
Local Traffic	Commercial District	Live Oak and NC-101



★
NPS Ferry

Wayfinding Strategy

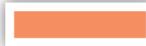


Routes

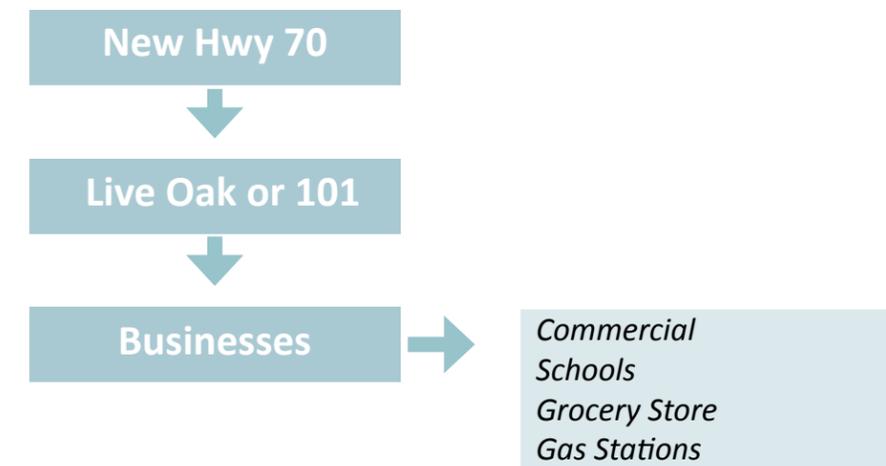
The Steering Committee and Public agreed that local traffic would use the Live Oak and 101 entries. Because many of the day to day necessities are located within the commercial district, it was agreed that Live Oak and NC-101 would be appropriate entrances for local traffic.

WHO NEEDS DIRECTIONS?	WHERE ARE THEY GOING?	HOW WILL THEY GET THERE?
Tourists	Historic District	Turner Street
Ferry go-ers	Ferry	Turner to Front Street
Local Traffic	Commercial District	Live Oak and NC-101



 Commercial District

Wayfinding Strategy



Signage

Once the wayfinding strategy or desired routing for users has been established for all major destinations, the next step in wayfinding is developing signage. Signage is a fundamental component to the success of the wayfinding strategies set forth for each Town Entry. The Steering Committee’s goal that all signs should have a sense of “Beaufortness” provides the foundation for developing a new wayfinding system for Beaufort. This Wayfinding Master Plan will help define the types of signage needed in Beaufort and the potential “look and feel” of those signs.

In order to assure the new wayfinding signage in Beaufort will have a sense of “Beaufortness”, the “Beaufortness” characteristics of existing signs needed to be defined. An extensive inventory and analysis of signs throughout Beaufort was performed and the Committee and the Public were asked to give input on which existing signs best reflected the Town’s character.

The following characteristics best define signs with a sense of “Beaufortness”:

Human Scaled

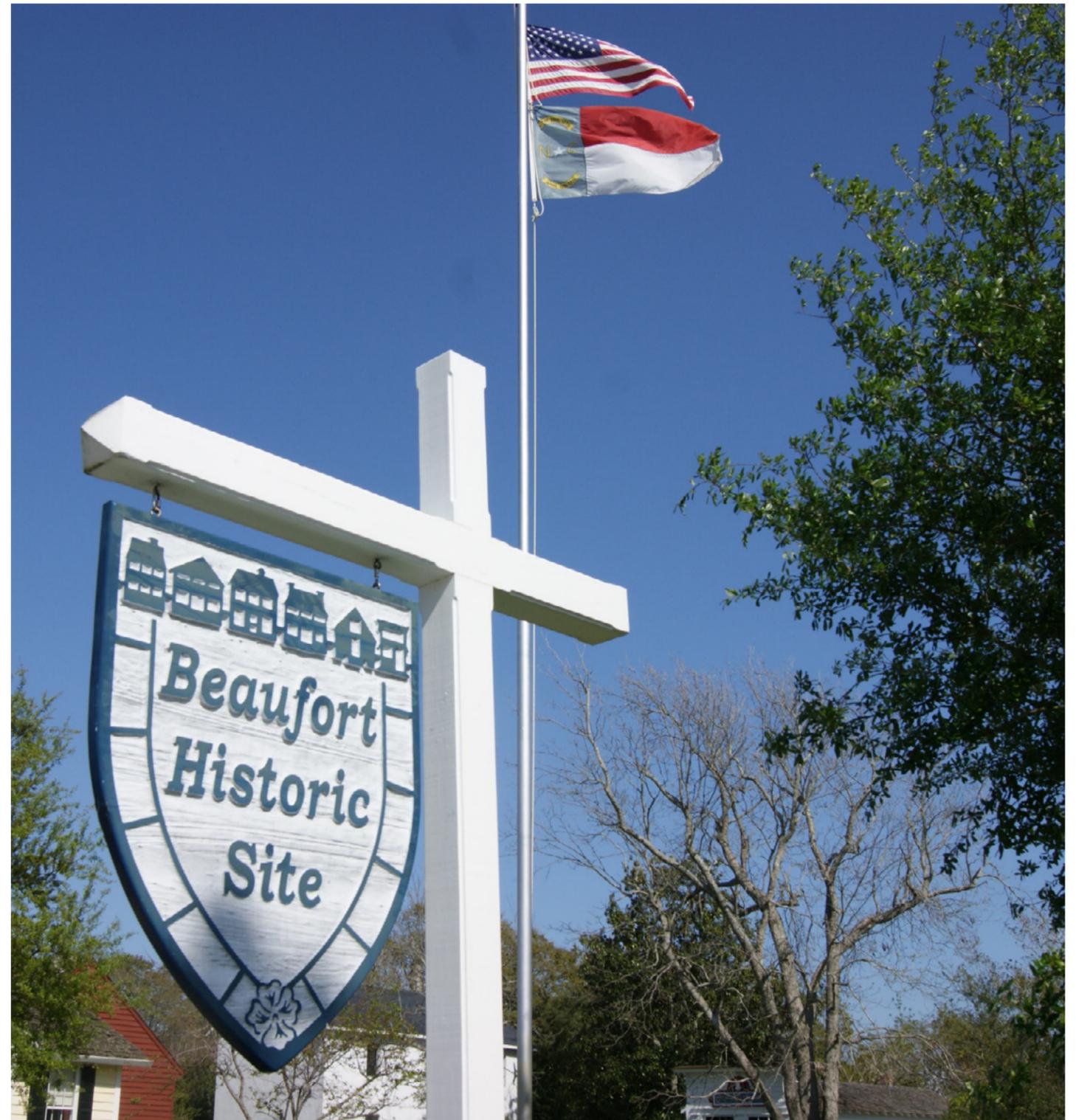
- Sized for the pedestrian*
- Accommodates drivers if needed*
- Simple-one sign for one reason*

Evolved Variety

- Varied Fonts*
- Varied shapes*
- Varied attachments*
- Varied*

Materials

- | | |
|---------------------|-------------------------|
| <i>Hand-crafted</i> | <i>Posts</i> |
| <i>Wood</i> | <i>Weathered</i> |
| <i>Paint</i> | <i>Modest detailing</i> |



Signage: WITHOUT a sense of “Beaufortness”

Signage is a fundamental component to the success of a wayfinding system. Below are existing signs in Beaufort the Public and Steering Committee felt best represented signs with a “sense of Beaufortness”

Human Scaled

*Sized for the pedestrian
Accommodates drivers if needed
Simple-one sign for one reason*



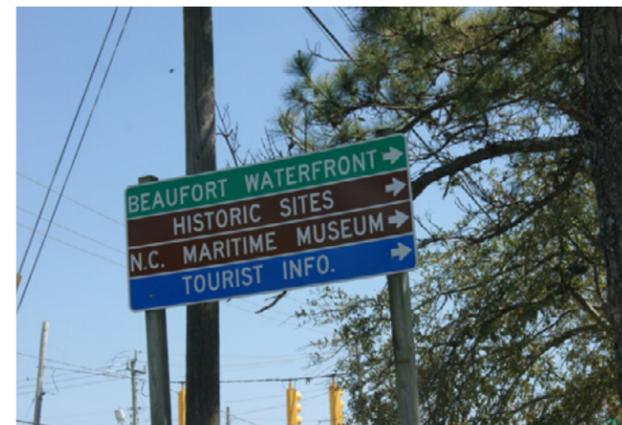
Evolved Variety

*Varied Fonts
Varied shapes
Varied attachments
Varied*



Materials

*Hand-crafted
Wood
Paint
Posts
Weathered
Modest detailing*



Signage: WITH a sense of “Beaufortness”

Below are existing signs in Beaufort the Public and Steering Committee felt best represented signs with a “sense of Beaufortness”.

Human Scaled

*Sized for the pedestrian
Accommodates drivers if needed
Simple-one sign for one reason*



Evolved Variety

*Varied Fonts
Varied shapes
Varied attachments
Varied*



Materials

*Hand-crafted
Wood
Paint
Posts
Weathered
Modest detailing*



Case Study: Getting to the Historic District

In order to illustrate the use of wayfinding signage at the new Beaufort Entries, a case study was performed for how drivers would be directed to the Historic District. The Committee and the Public agreed that visitors to the Historic District should be directed to the Turner Street Entry.

However, finding Turner Street is only one step in finding their ultimate destination. This case study will describe the strategy for finding destinations in the Historic District and will illustrate potential signs to help visitors navigate the new entry.

The preferred wayfinding strategy for visitors to the Historic District is for visitors to make their way to Turner Street and drive into the District. Once in the District, they would be directed to parking. Once visitors park their cars, pedestrian signage would direct them to their destination (s).

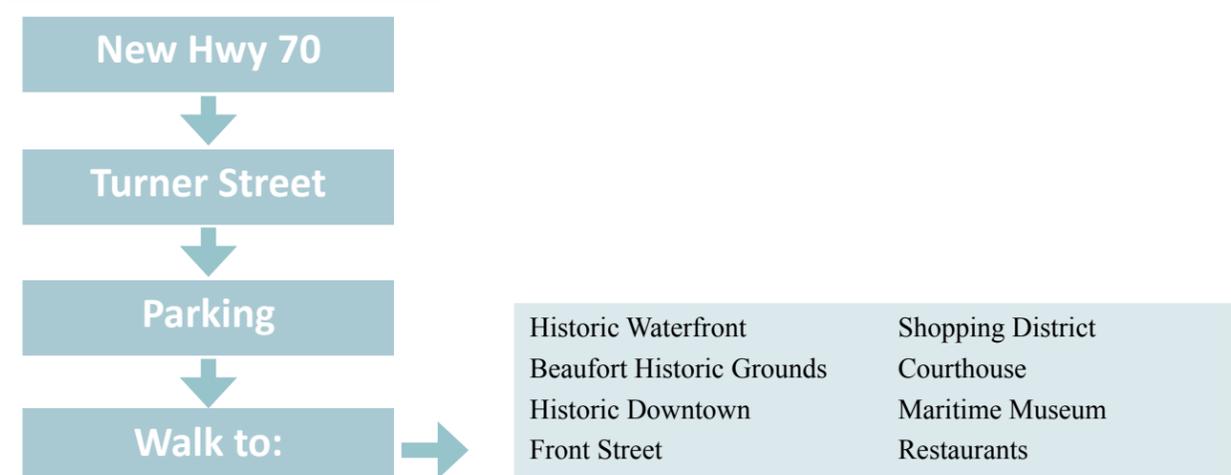
To assure visitors find their destinations, different types of signage will be needed to provide visitors with the necessary information they need to make a decision. This signage must help visitors make five (5) key decisions in order to ultimately find their destination within the District.

Key Decisions for Drivers:

- 1 *Where do I turn off of Hwy 70 to access the Historic District?*
- 2 *When do I know I have entered the Town of Beaufort?*
- 3 *When do I know I have entered the Historic District?*
- 4 *How will I find parking?*
- 5 *How do I walk to my destination?*



Wayfinding Strategy



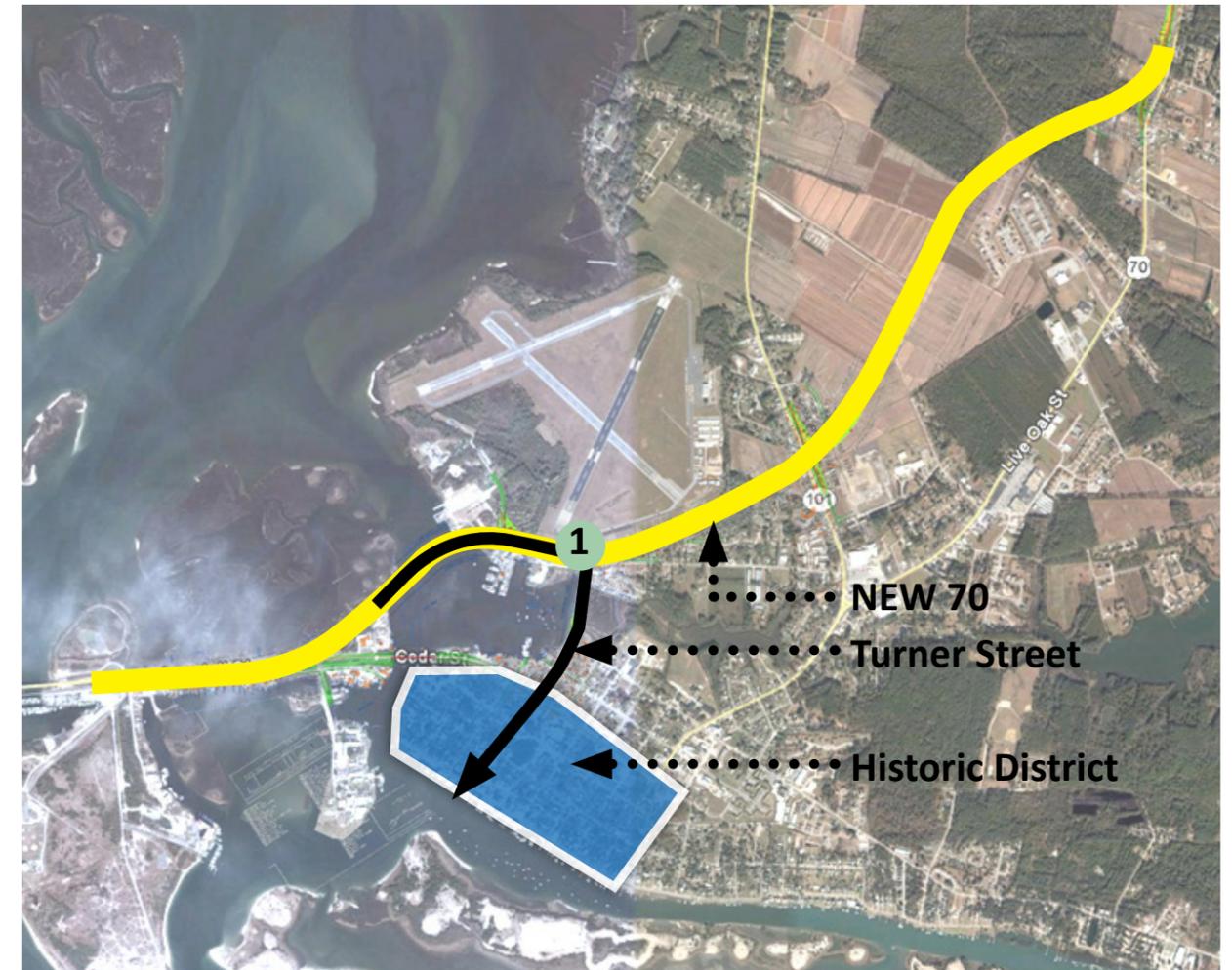
1 Where do I turn off of Hwy 70 to access the Historic District?

Drivers will first experience NCDOT signage on the New Hwy 70 which will begin to direct them to Beaufort. These signs must meet the strict Federal Highway Administration’s signage standards from the Manual of Uniform Traffic Control Devices (MUTCD).

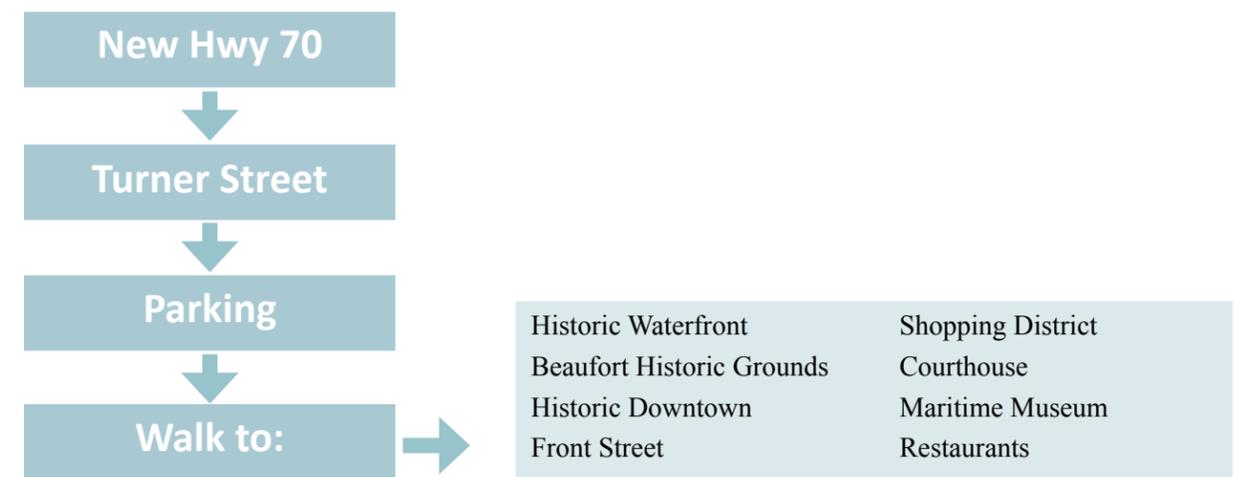
However, we would recommend installing the NCDOT standard sign panels on “Beaufort Posts” which reflect the scale, material and character of signage posts found in Beaufort today. These posts would be required by NCDOT to have “breakaways” at the base of each post.



“Beaufort Post”
 The Beaufort Post is a white painted square wooden post. It extends beyond the top of the sign exposing a pyramidal cut post top.



Wayfinding Strategy



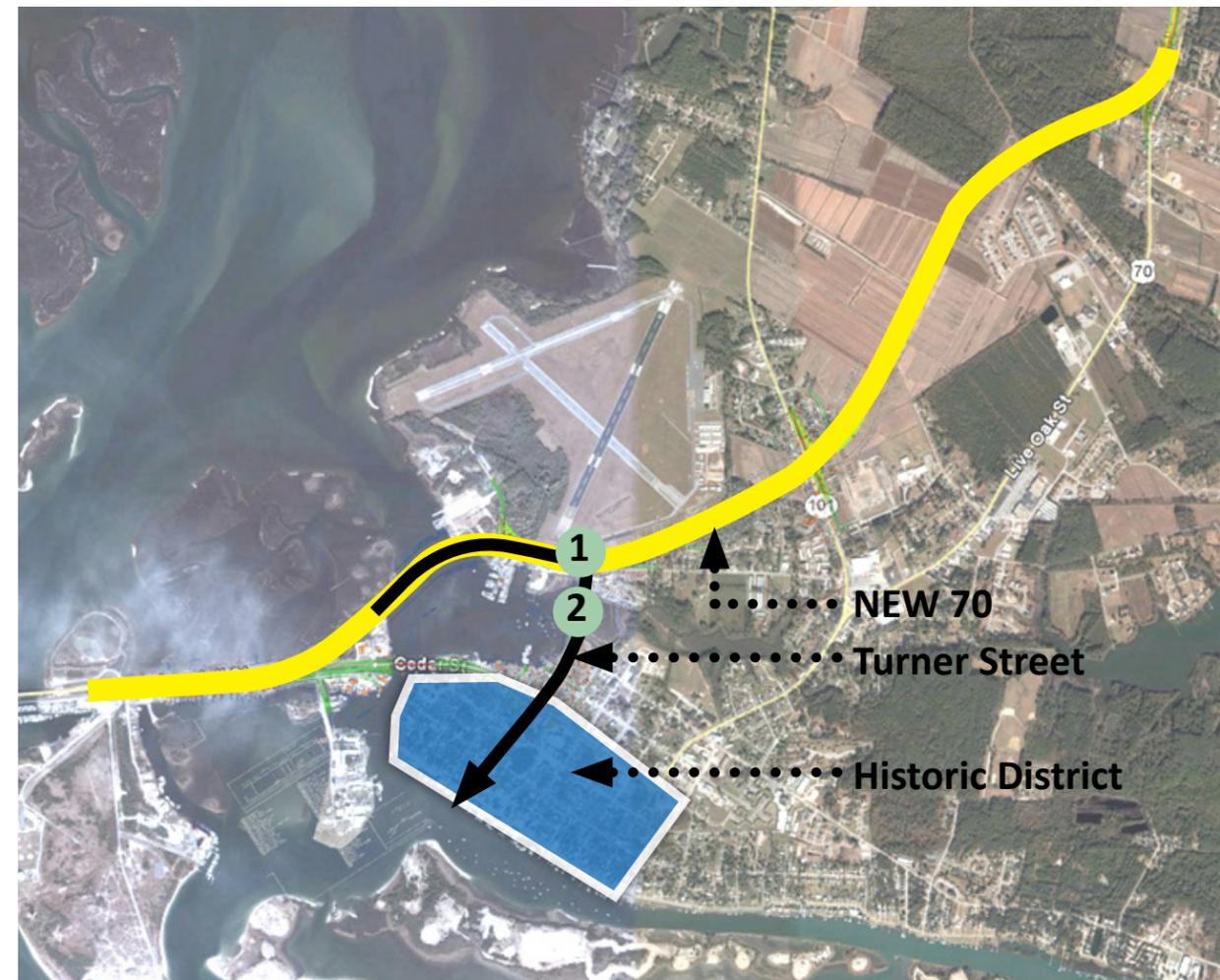
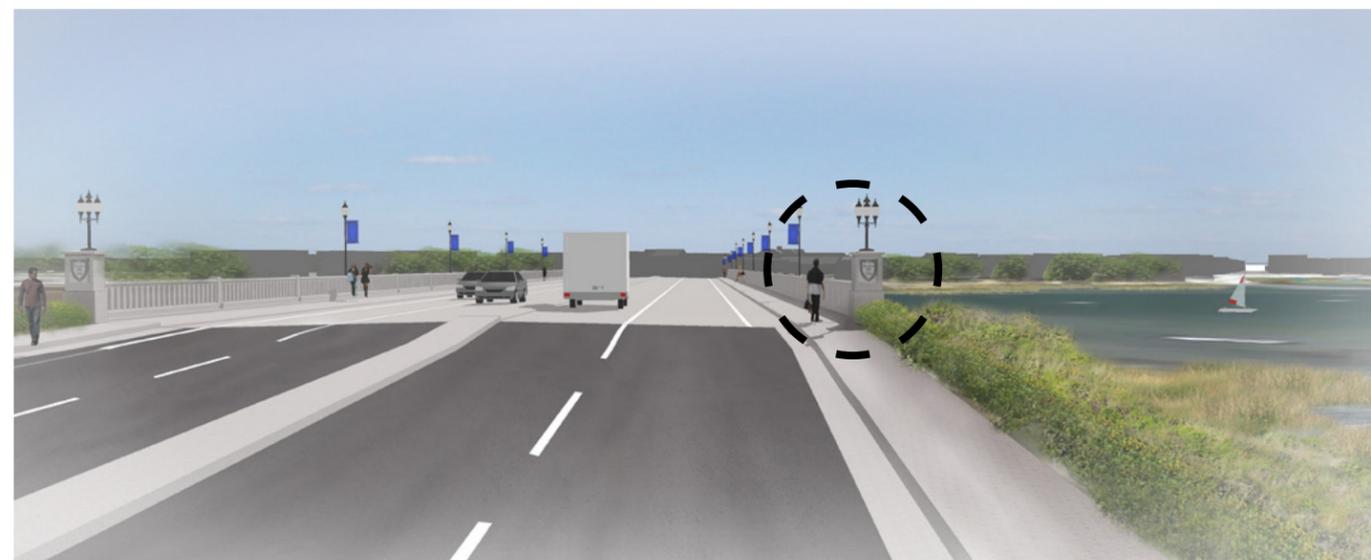
2 When do I know I have entered the Town of Beaufort?

A consistent wayfinding element should be used at all of the Town Gateways which have been designated at the New Hwy 70 intersections at Turner, 101 and Live Oak.

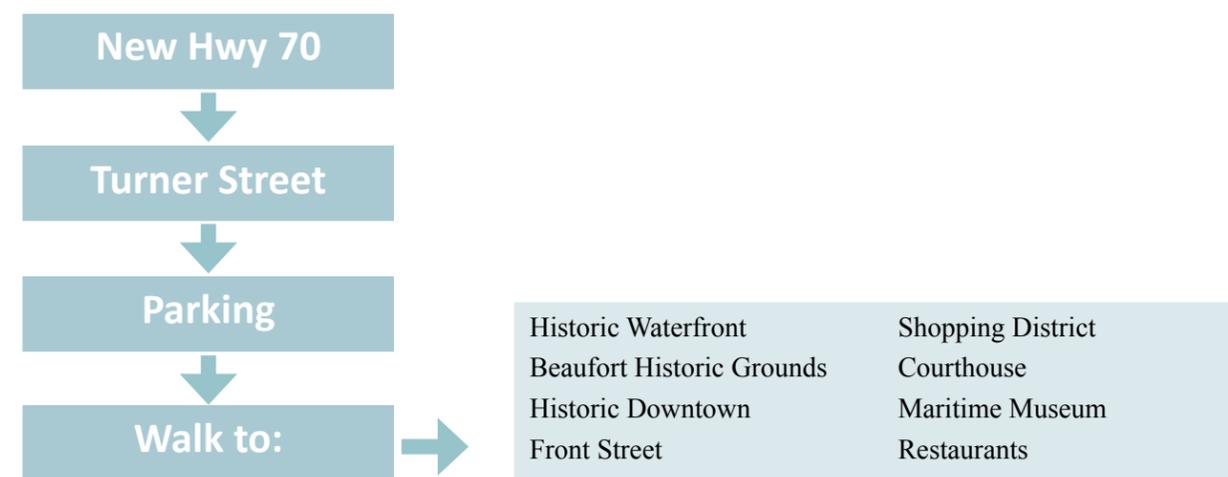
This wayfinding element should perform two (2) major functions:

- Provide a sense of place to assure visitors know they have arrived in Beaufort
- Provide a sense of entry into Beaufort similar to the feeling one would get when they would have entered the old Beaufort gates.

We recommend using the “Beaufort Gateway Column” as the Town Marker. This column will perform the above functions by creating a prominent, yet tasteful visual marker at all Beaufort Entries. The column would be precast concrete to match the railings on the Turner Street Bridge. Standing about 8’ tall, the column would contain the Beaufort shield within a recessed panel. The column would provide a sense of place and a visual relationship to the rich history that has made the Town the wonderful destination it is today.



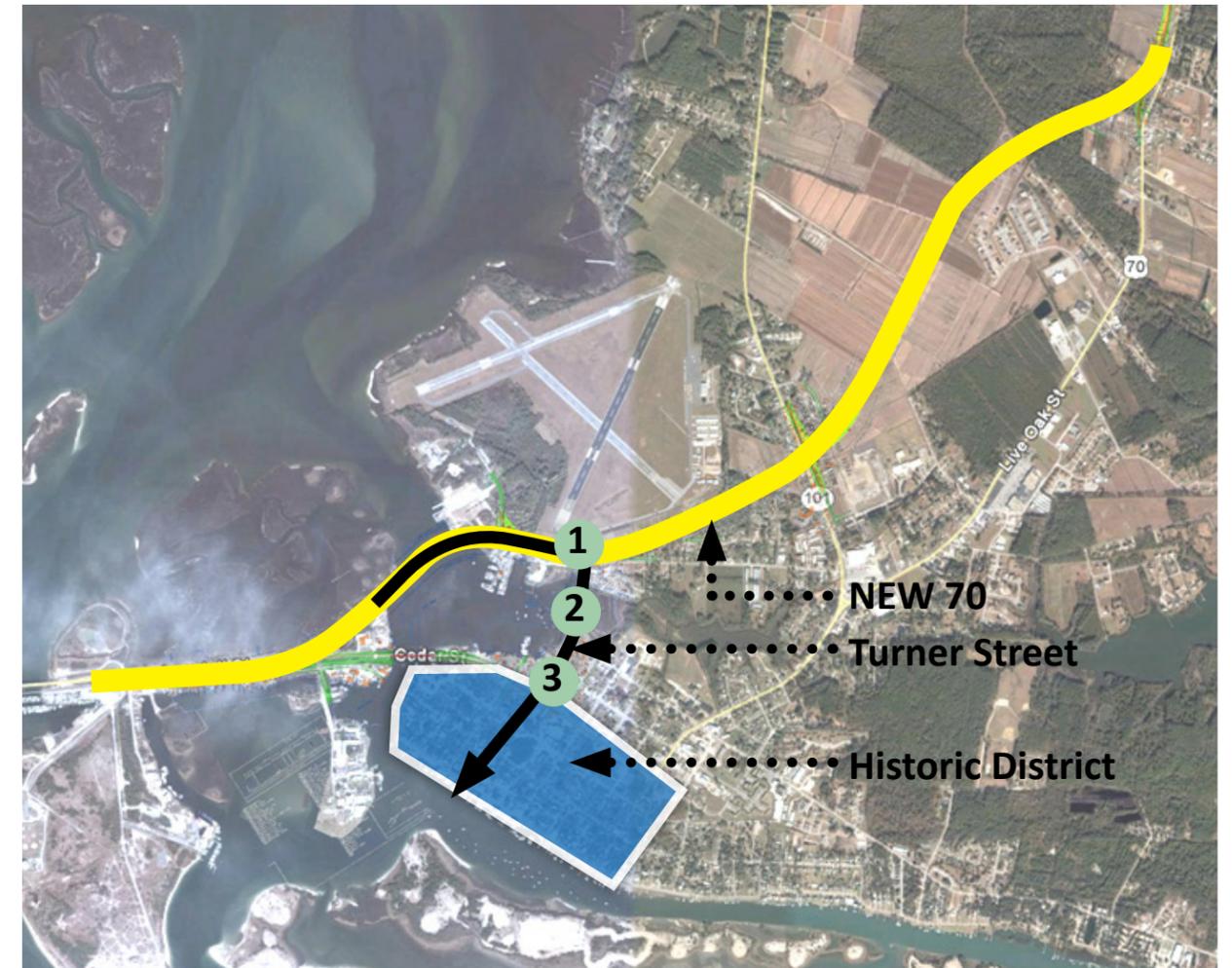
Wayfinding Strategy



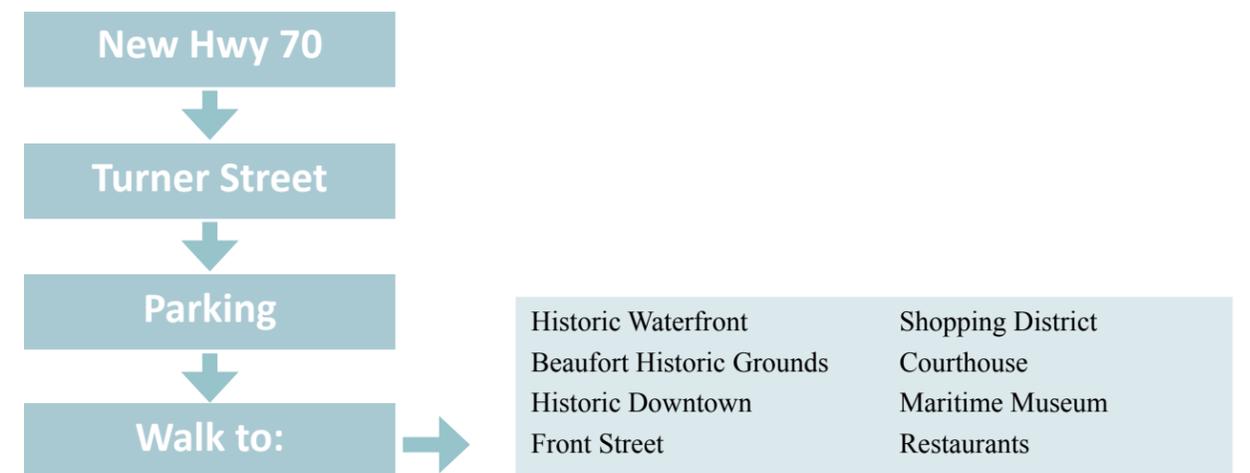
3 When do I know I have entered the Historic District?

Once visitors have entered the Town of Beaufort, they will wonder “how do I get to the Historic District?”. Designating the entrance to the Historic District is a very important wayfinding component that welcomes visitors to this special area of Beaufort. The Committee and the Public agreed the existing Historic District Entry is suitable for the Town.

We recommend using the existing Historic District Entry sign or a sign of similar character. The stone supports of the existing sign should be modified to fit into the material character of the Historic District. We recommend hanging the sign from white ‘Beaufort Posts’. The location of the sign would be moved to the district gateways at Turner and Cedar as well as Cedar and Live Oak intersections.



Wayfinding Strategy



4 How will I find parking?

The Historic District and waterfront area are extremely walkable parts of the Town. While the scope of this Master Plan does not include a detailed parking analysis, it does acknowledge that parking in the District is the most critical component to the sustained success of Beaufort.

Wayfinding signage is a necessary element to assuring visitors can readily find parking within the District and the Town already understands this as evidence by the existing parking signage.

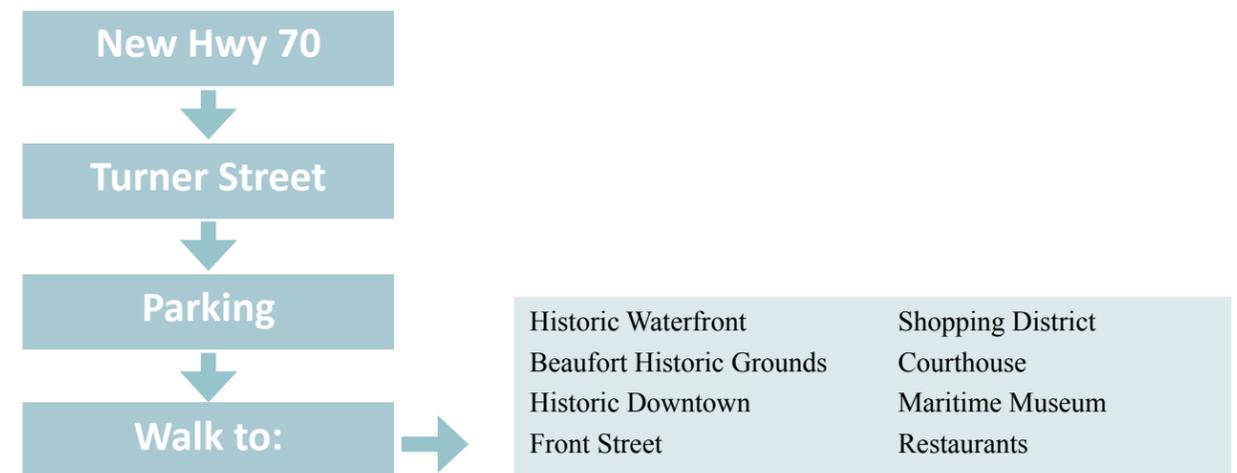
The current signs consist of a metal sign panel with vinyl graphics that complement the character of the Historic District. The signs are mounted on an unpainted square wooden post.

The current signs reflect the “Beaufortness” characteristics of signs found in the District, thus we would recommend the continued use of this sign. We would suggest the following modifications to the signage materials because the current materials do not match the signage materials preferred by the Committee or the Public.

The sign panel should reflect a hand-crafted sign, preferably with routed wood. The post should be painted white and match the post style of the Historic Sites Signage.



Wayfinding Strategy



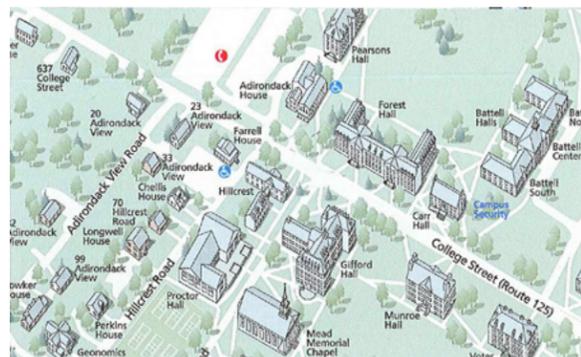
5 How do I walk to my destination?

Once visitors have parked their cars, they are now ready to make their way to their destinations, whether it be a restaurant, shops or the Historic Sites. Because the Historic District and waterfront area are compact and walkable, there is not a need for an overwhelming amount of pedestrian directional signage within the Town. As one Committee member stated, “Beaufort is not a bad place to get lost”.

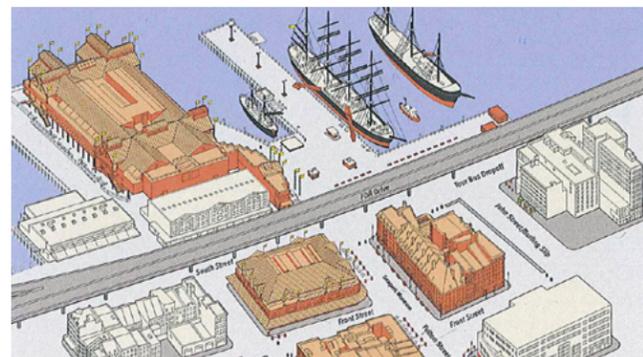
However, a limited amount of pedestrian directional signage would be appropriate to allow visitors to orient themselves upon arrival.

We would recommend providing two (2) types of pedestrian directional signage:

A) District Map or Walking Guide

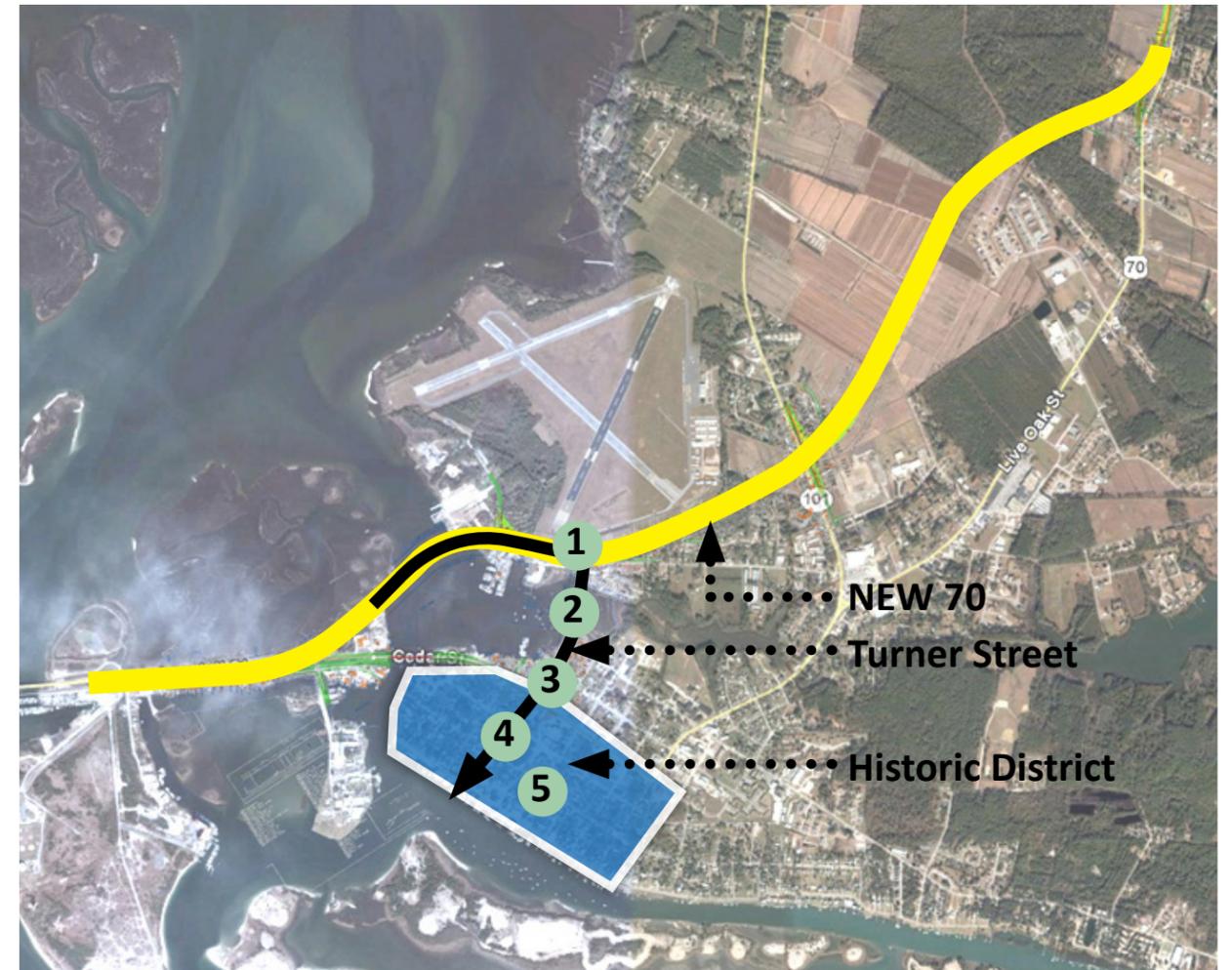


images source: “The Wayfinding Handbook” by David Gibson

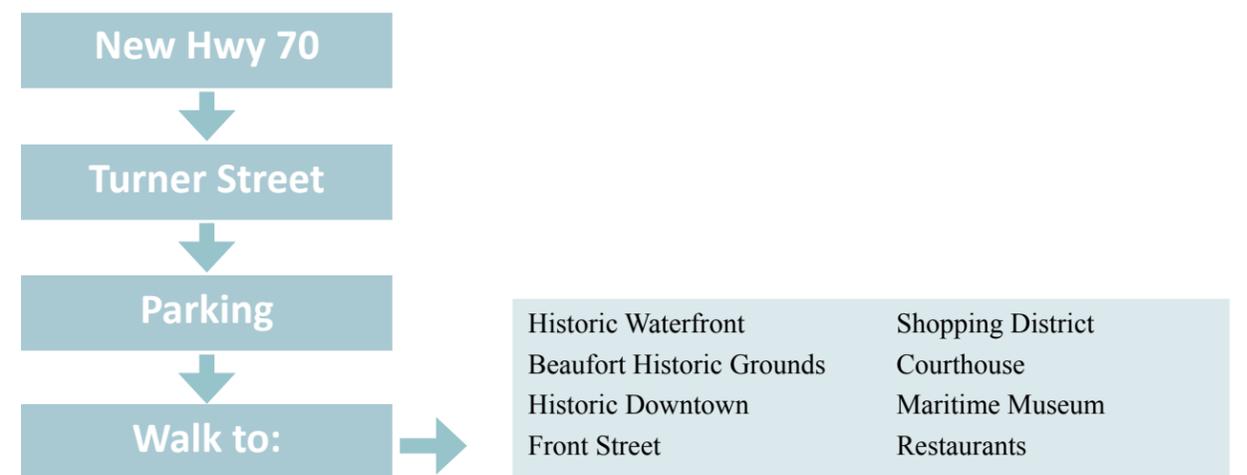


B) Trailblazer Signs

Useful in key areas to assure visitors begin walking in the correct direction to travel to their destination.



Wayfinding Strategy



Signage: Regulatory Signage

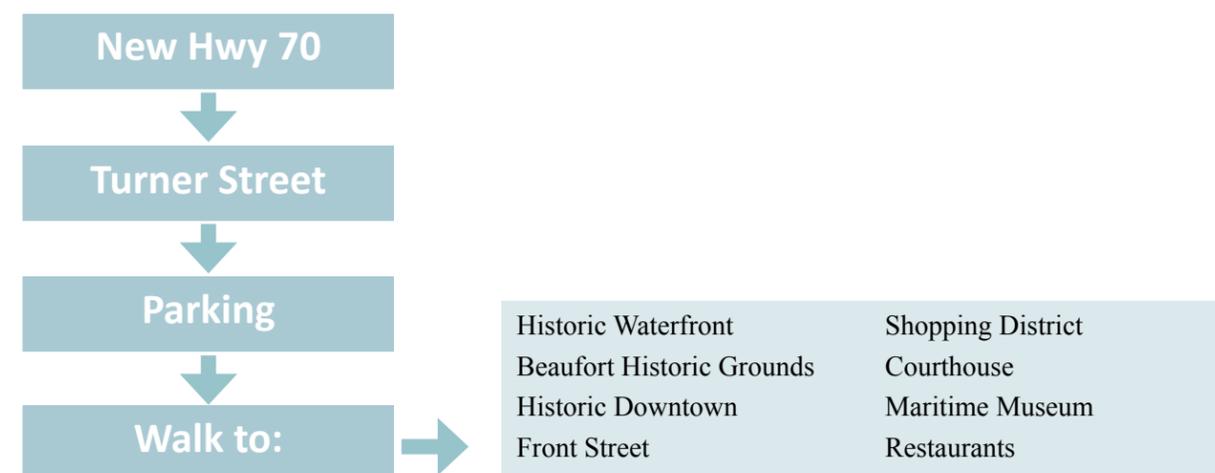
How can regulatory signage reflect “Beaufortness”?

Regulatory signage, or signs that instruct road users on what they must or should do (or not do) under a given set of circumstances, are regulated by State and Federal signage standards. Regulatory signs (stop, yield, speed limit and others) within Beaufort must adhere to these standards, but there are some opportunities for modifications in order to help reinforce the feel and character of Beaufort.

We would recommend installing regulatory sign panels on “Beaufort Posts” which reflect the scale, material and character of signage posts found in Beaufort today. A blue background panel to match the blue of existing Beaufort signs could be added to unify regulatory signs with other wayfinding elements.

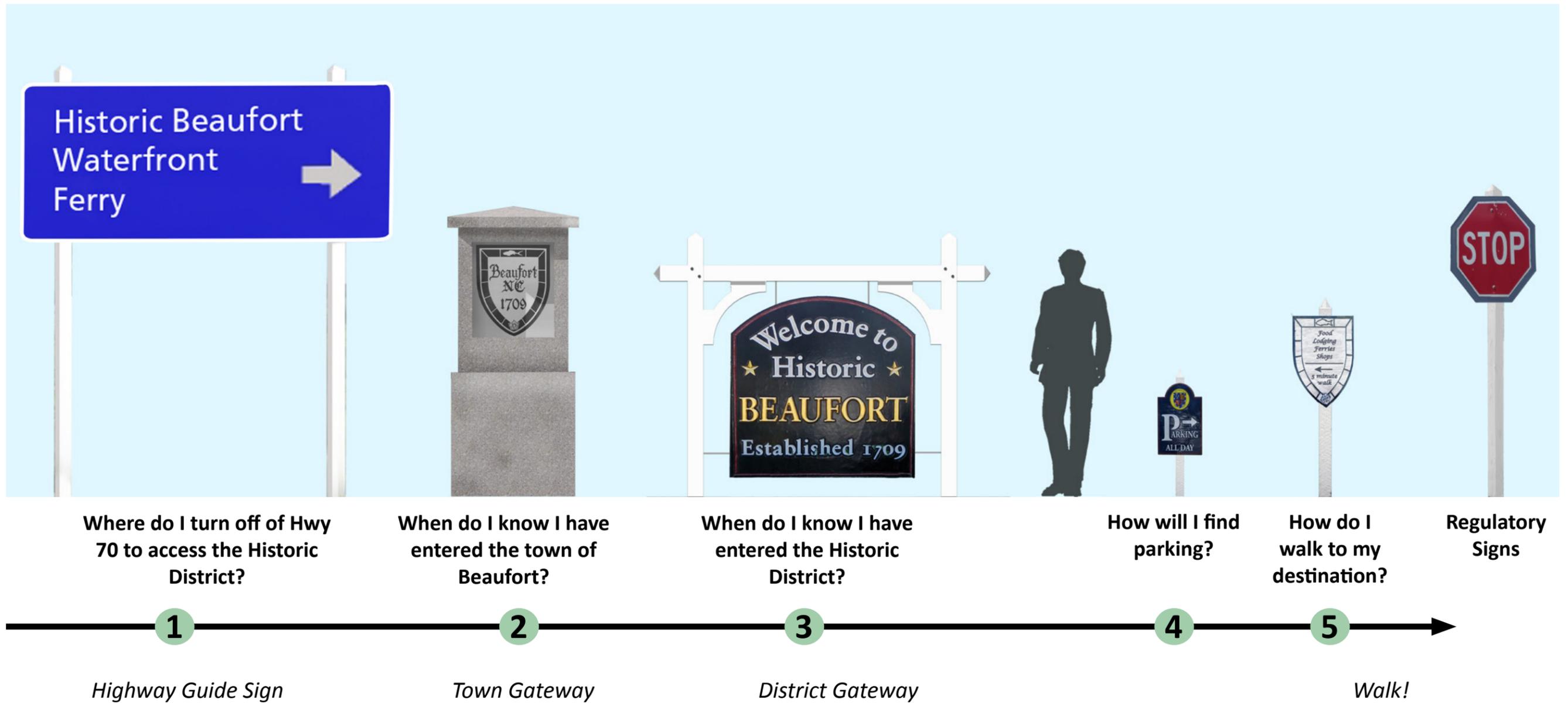


Wayfinding Strategy



The Potential Family of Signs

The signs below represent the potential “Beaufort Wayfinding Signage Family”. Each sign serves a different wayfinding purpose and assists visitors with navigating Beaufort. Each sign complements the other, while still providing variety and distinct elements that reflect “Beaufortness”.



4 Section 4 IMPLEMENTATION AND ACTION

Implementation Strategies

Implementation of this Master Plan will require a significant commitment on the part of the citizens of the Town of Beaufort and Carteret County. This commitment, however, will not be only monetary. A variety of community resources will need to be marshalled in support of this effort to ensure its ultimate success. The involvement, support and financial participation of many public entities, private corporations, civic groups, and individuals are necessary if this plan is to be anything more than a dream.

So, where to go from here? The answer has many parts as the needs and opportunities are plentiful. In general terms, action needs to be taken on five separate, but related activities. They include:

- 1 Public Awareness/ Education
- 2 Design
- 3 Fund-Raising
- 4 Maintenance
- 5 Regulatory Controls/Enforcement

Each of these major headings has a number of action items that need to be addressed in the near future. A brief description of these activities follows.

Public Awareness / Education

At the time of the preparation of the Master Plan, the Beaufort Entry Master Plan has been enjoying widespread support and positive public exposure. This effort needs to be expanded to the widest possible extent so that as many citizens as possible understand it and most importantly, see the benefits. Reasons for the need of this support go far beyond the willingness to provide funding. In order for the entry corridors to reach their ultimate goals, attitudes and perceptions will need to be changed. These considerations include such obvious things as personal property clean up, coordinated utilization of signs, painting existing sign posts white, and small landscape improvements. Businesses and individuals can participate in the Beaufort Entry enhancement program without major effort or expense simply by being aware of what is going on.

Furthermore, it will be important to make as many people as possible aware of all progress that is made. Every body wants to be associated with successful projects and everybody likes to receive recognition for efforts on behalf of the community. The opportunity exists with the Beaufort Entry project to build support and momentum very quickly with a timely and well orchestrated public-awareness program. It is strongly recommended that a public relations firm be considered to develop a program that can be launched at the appropriate time.

Design

A series of design and design-related efforts need to be undertaken quickly to insure the continued forward momentum that the Beaufort Entry project is currently enjoying. Several immediate action items that will have the greatest immediate impact will be discussed in the immediate action plan. This plan should be developed as the next step once the Master Plan is adopted. Conducting a traffic analysis to test roadway design recommendations will be critical to moving the project forward. Immediate discussion with NCDOT is critical to the design of the Turner Street Bridge, lighting, rail and column design.

It is critical that pilot projects be undertaken by the town to encourage the private sector to participate on private property. These pilot project need not be extensive. However, based on funding, the pilot projects should be done well no matter the size. Stretching funds to the extent that improvements are hardly noticed will be a disincentive to the private sector and the town will receive little credit for expenditure of public funds. The key for pilot projects and design is quality, no matter what size the project is.

As other specific projects are identified and funds made available, detailed design and construction documents will need to be prepared to varying degrees depending on the project. It cannot be overemphasized that in order for the proposed improvements to have the maximum benefit over a long period of time, the designs have to be well thought out and implemented by trained experienced professionals. Otherwise, the proposed improvements will, in a short time, become negative contributors to the visual environment.

Fund-Raising

Obviously, the commitment of adequate funding will be essential in the successful implementation of the Master Plan. Substantial financial resources are going to be required for major improvements to be implemented. It should be pointed out early and restated often that this cannot and should not be a publicly funded project only. Every business, public entity, and individual stands to gain some benefit from the successful implementation of this program, and everyone should be willing to participate on some level, no matter how small.

Corporations and other institutions need to accept responsibility for any frontage they may have, and those without frontage need to “adopt” specific project or elements for sponsorship. Civic clubs, church groups, schools, and other similar groups need to participate through in-kind support, special projects, and maintenance activities.

The town should consider sponsorship of fund-raising activities which afford individuals, families, civic groups, neighborhoods, etc., the opportunity to “buy-a-tree” or “light-a-lamp”. These types of programs enable everyone to participate as their resources will allow, and to see concrete physical results from their contribution. Everyone has the chance to buy into the concept and receive recognition for their gift. Formation of a non-profit corporation or foundation may be necessary to adequately manage and distribute funds raised in such a manner.

Lastly, groups such as Duke Energy / Progress Energy, Time Warner Cable, and North Carolina Department of Transportation need to be approached with specific requests for participation and capital improvement projects that relate to their facilities. Duke Energy and other utilities need to be convinced of the need for underground and realignment of overhead wires. NCDOT has several programs, including the Complete Streets Program, which could be used to leverage landscaping at highway interchanges, pedestrian and bikeway improvements, median plantings, and signalization improvements.

Maintenance

Nothing will stop this project faster than poor maintenance of highly visible entry corridor improvements. Ongoing maintenance activities will be an enduring part of every project that is undertaken. It too will be a shared responsibility of both public and private sectors. As mentioned earlier, budgets for annual maintenance activities will need to become routine and not subject to arbitrary elimination. The capital improvements being proposed are substantial. Proper maintenance is insurance that those funds are spent wisely.

As monies are solicited for specific projects or elements, a portion should be allocated for ongoing maintenance and set aside in a managed fund to insure its availability at the time it will be needed. Certainly opportunities will exist for “adopt-a-highway” type programs to be a part of the overall maintenance of the entry corridors. However funds will be needed to support even the most basic types of activities. It is recommended that no projects be undertaken that do not have some funds committed and set aside for maintenance functions.

Regulatory Controls

Some elements of the master plan only ordinances, codes or zoning requirements can address. Obvious ones would include: more strict control of on-site signage, restricted land use types for each corridor and screening of service and storage areas. Other issues that may need to be addressed by government control are depth of landscape areas, specific landscape requirements, and provisions for compliance of non-conforming properties.

It is recognized that these points may to some extent be currently addressed by existing codes and ordinance, but it is the recommendation of this Master Plan that a thorough review of all such applicable controls be undertaken to establish enforceability concerns, identify conflicts, and suggest revisions or amendments to such controls through design focused ordinances

It is the recommendation of the plan that a thorough study be undertaken to determine if an appropriate overlay district for the entry corridors would be beneficial. Business improvement districts and tax increment financing should be considered as fund raising strategies. If used properly they can be a positive influence and long term aid in promoting visually attractive development and compliance with the overall goals and objectives of this master plan. It may be appropriate to establish a district where the requirements apply only to undeveloped lands or project renovations as a tool for implementation.



East Boulevard in Charlotte - Example of NCDOT complete street, including bike lanes, planting areas, sidewalks, and street trees.

Source: Complete Streets - Flickr

Immediate Action Plan

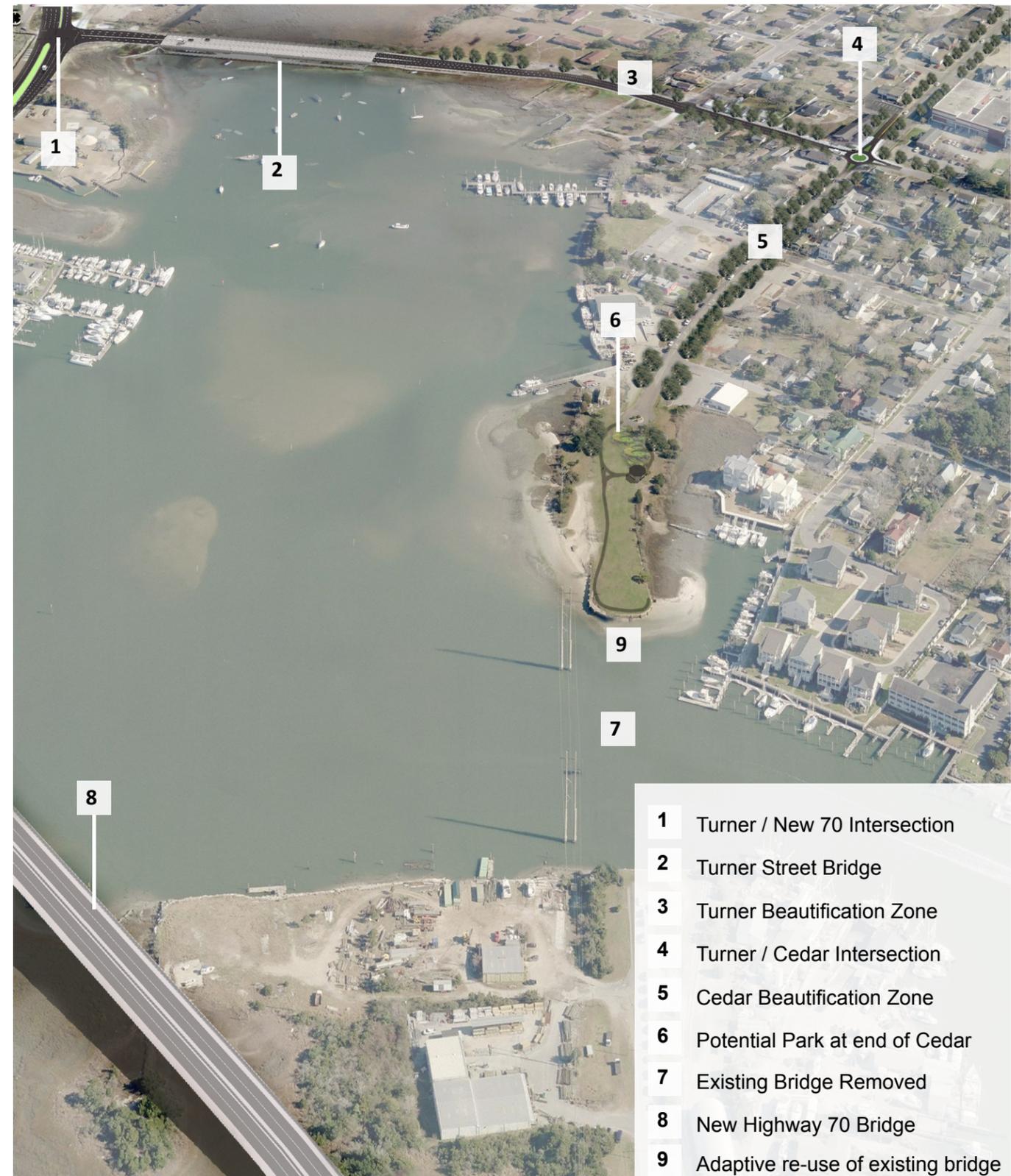
The following activities are action items from the recommendations of this master plan. The list is a result of guidance from the public and the steering committee. The activities are divided into short and long-term items.

Short Term Action Items

1. Identify funding sources for corridors and intersection improvements.
2. Discuss recommendations with NCDOT to determine an implementation strategy
3. Conduct a Traffic Study to validate roadway recommendations
4. Determine NCDOT's Landscape budget and get it transferred to town
5. Sense of place elements - painted white posts, lights, landscaping , fences, signage
6. Develop Utility Relocation Plan
7. Develop policies related to this Master Plan and include in future comprehensive plan
8. Consider ordinance text amendments and land use plans to facilitate Master Plan goals and objectives
9. Live Oak / 101 intersection (first major project to illustrate the principles of the beautification master plan)
10. Walking and wayfinding (Cedar into the district - ferry-goers)
11. Parking (town is looking into short, mid and long term solutions)
12. Front street beautification - lights signs, trees , trash receptacles

Long-Term Action Items

1. Begin discussion with power company related to relocation of power lines underground
2. Appropriate funding and develop Request for Proposals for the development of the park at the west end of Cedar Street



5 Section 5 APPENDIX

Presentations

- Kick Off Presentation
- Analysis Presentation
- Public Meeting 1
- Public Meeting 2

Meeting Minutes

- **Kick Off Meeting**
- **Analysis Meeting**
- **Public Meeting 1 Recap**
- **Public Meeting 2 Recap**

